

RAILROAD RESTORATION AGENCY CREEK DRAFT FINAL DESIGN

Clatsop County, Oregon
February 2024

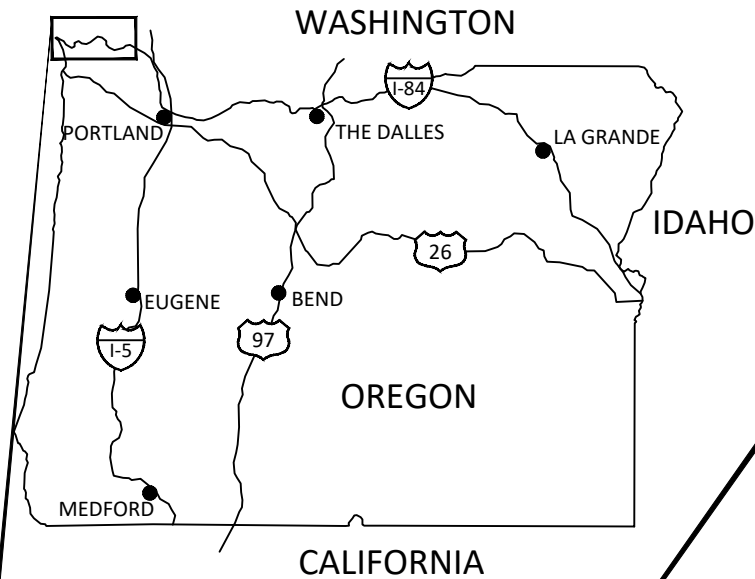
COORDINATES:

AGENCY CREEK

LATITUDE : 46°11'10" N
LONGITUDE 123°36'27" W

TOWNSHIP 8N, RANGE 7W,
SECTION 7 & TOWNSHIP 8N,
RANGE 8W, SECTION 12

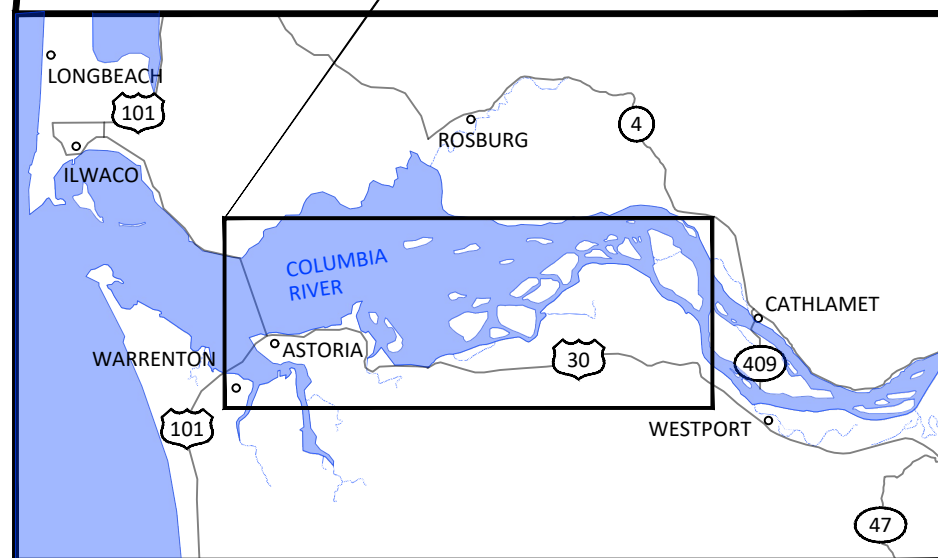
THIS PROJECT WAS DESIGNED IN ACCORDANCE WITH THE BPA HABITAT IMPROVEMENT PROGRAM, PROGRAMMATIC BIOLOGICAL OPINION (HIP).



LOCATION MAP
STATE OF OREGON
NOT TO SCALE



SITE MAP
NOT TO SCALE



VICINITY MAP
NOT TO SCALE

Sheet List Table

1	COVER, VICINITY MAP, AND SHEET INDEX	18	SITE ACCESS & DETAILS	37	BRIDGE STANDARDS - EMBEDDED PLATES
2	GENERAL NOTES (1 OF 2)	19	TEMPORARY FILL ACCESS DETAILS	38	BRIDGE STANDARDS - DECK AND CURB PLATES
3	GENERAL NOTES & QUANTITIES (2 OF 2)	20	REVEGETATION PLAN	39	BRIDGE STANDARDS - CURB AND WALK MISC.
4	HIP GENERAL CONSERVATION MEASURES (1 OF 3)	21	REVEGETATION DETAILS (1 OF 2)	40	BRIDGE STANDARDS - REAR BENDING DIAGRAM
5	HIP GENERAL CONSERVATION MEASURES (2 OF 3)	22	REVEGETATION DETAILS (2 OF 2)		
6	HIP GENERAL CONSERVATION MEASURES (3 OF 3)	23	BRIDGE STANDARDS - TITLE PAGE		
7	EROSION CONTROL DETAILS	24	BRIDGE STANDARDS - GENERAL NOTES		
8	EXISTING CONDITIONS & SURVEY CONTROL	25	BRIDGE STANDARDS - TYPICAL ELEVATIONS		
9	TEMPORARY ACCESS	26	BRIDGE STANDARDS - BENT CAP		
10	PROPOSED CONDITIONS OVERVIEW & INDEX	27	BRIDGE STANDARDS - ABUTMENT CAP		
11	PROPOSED CONDITIONS LEVEE A	28	BRIDGE STANDARDS - 20 INCH WING WALL		
12	PROPOSED CONDITIONS LEVEE B	29	BRIDGE STANDARDS - WINGWALL		
13	PROPOSED CONDITIONS LEVEE C	30	BRIDGE STANDARDS - 20 INCH SLAB BEAM		
14	PROPOSED CONDITIONS OPENING	31	BRIDGE STANDARDS - 20 INCH SLAB BEAM		
15	BRIDGE SPAN PROFILE AND BANK PROTECTION SECTION	32	BRIDGE STANDARDS - 30 INCH DOUBLE CELL BOX BEAMS		
16	BRIDGE PLAN AND ELEVATION	33	BRIDGE STANDARDS - SLOPED CURB AND STRAND PATTERN		
17	BRIDGE TYPICAL DETAILS	34	BRIDGE STANDARDS - HAND RAIL STANDARDS		
		35	BRIDGE STANDARDS - LIFTING DETAILS		
		36	BRIDGE STANDARDS - PILE SPLICE		

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NO.	BY	DATE	REVISION DESCRIPTION

GS	MC, CA, MB	MB
DRAWN	DESIGNED	CHECKED
MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



501 Portway Avenue, Suite 101
Hood River, OR 97031
541.386.9003
www.interfluve.com

COVER, VICINITY MAP, AND
SHEET INDEX

SHEET

1 OF 40

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THE CONTRACTOR SHALL ATTEND A MANDATORY PRE-BID SITE MEETING.

THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION MEETING WITH COLUMBIA RIVER ESTUARY STUDY TASKFORCE (CREST, OWNER) PRIOR TO BEGINNING CONSTRUCTION.

ALL WORK SHALL BE GOVERNED BY THE GENESEE AND WYOMING STANDARD SPECIFICATIONS AND PUBLIC PROJECT MANUAL.

ADDITIONALLY, WORK ON THIS PROJECT SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE 2021 OREGON DEPARTMENT OF TRANSPORTATION (ODOT) STANDARD SPECIFICATIONS.

IF ANY PORTION OF THESE REFERENCE CONDITIONS ARE IN CONFLICT WITH EACH OTHER, THE DOCUMENTS THAT GOVERN SHALL ADHERE TO THE FOLLOWING ORDER OF PRECEDENCE:

1. GENESEE AND WYOMING STANDARD CONSTRUCTION SPECIFICATIONS
2. AMERICAN RAILWAY ENGINEERING AND MAINTENANCE OF WAY ASSOCIATION MANUAL FOR RAILWAY ENGINEERING (AREMA MANUAL)
3. ODOT (2021) STANDARD SPECIFICATION AND SPECIAL CONDITIONS MODIFYING THE STANDARD SPECIFICATIONS.

EXISTING DATA

TOPOGRAPHIC AND BATHYMETRIC SURVEY DATA WERE COLLECTED BY INTER-FLUVE, INC & CREST STAFF USING TOTAL STATION, RTK GPS AND ECOSOUNDER SONAR EQUIPMENT ON JUNE 18 & 19, 2019, AND APRIL 29, 2021. THESE DATA ARE REFERENCED TO:
 HORIZONTAL DATUM: NAD83 OREGON STATE PLAN, NORTH ZONE
 VERTICAL DATUM: NAVD88
 UNITS: INTERNATIONAL FEET

LIDAR DATA OBTAINED FROM THE LOWER COLUMBIA ESTUARY PARTNERSHIP, COMMISSIONED BY THE US ARMY CORPS OF ENGINEERS, FLOWN BETWEEN DECEMBER 2, 2009 AND FEBRUARY 22, 2010 WAS USED TO SUPPLEMENT TOPOGRAPHIC SURVEY DATA TO DEVELOP DIGITAL ELEVATION MODELS.

PROPERTY BOUNDARIES SHOWN ARE FROM THE CLATSOP COUNTY TAXLOT GIS LAYER.

TIDAL DATUMS AND WATERS BOUNDARIES

TIDAL DATUMS DISPLAYED IN THIS PLANSET ARE REFERENCED TO NAVD88.

HIGHEST MEASURED TIDE (HMT) = 12.56' - RECORDED AT THE TONGUE POINT STATION (#9439040).

THE DATUMS BELOW ARE CALCULATED BY INTERPOLATION BETWEEN THE TONGUE POINT STATION (#9439040) & THE WAUNA STATION (#9439009) BASED ON LOCATION OF PROJECT SITE AS REFERENCED BY NAUTICAL RIVER MILE (NM) ALONG THE COLUMBIA RIVER.

AGENCY CREEK (NM 22)

ORDINARY HIGH WATER (OHW) = 9.97'

MEAN HIGHER HIGH WATER (MHHW)/ HIGH TIDE LINE = 8.88'.

MEAN HIGH WATER (MHW) = 8.22'.

MEAN LOWER LOW WATER (MLLW) = 0.73'.

THESE DO NOT NECESSARILY REPRESENT JURISDICTIONAL BOUNDARIES. WITHIN THE STATE OF OREGON, THE ARMY CORPS OF ENGINEERS AND THE DEPARTMENT OF STATE LANDS HAVE THE FINAL AUTHORITY IN DETERMINING WATERS AND WETLANDS BOUNDARIES AND REGULATIONS.

SOILS

SOILS WITHIN THE PROJECT SITE CONSIST MAINLY OF COQUILLE-CLATSOP COMPLEX, 0 TO 1 PERCENT SLOPES. SOME HUMITROPEPTS, 25 TO 60 PERCENT SLOPES ARE ALSO PRESENT ALONG THE RAILROAD PRISM AS MAPPED BY NRCS.

BPA HIP

THIS PROJECT WAS DESIGNED IN ACCORDANCE WITH THE BPA HABITAT IMPROVEMENT PROGRAM, PROGRAMMATIC BIOLOGICAL OPINION (HIP). HIP GENERAL CONSERVATION MEASURES (CMs) ARE INCLUDED ON SHEETS 4 5 AND 6. SITE SPECIFIC DIRECTION IS INCLUDED IN THE FOLLOWING GENERAL NOTES. IN CASE OF A CONFLICT BETWEEN THE REGULATORY STANDARDS OR SPECIFICATIONS, THE MORE

STRINGENT WILL PREVAIL, UNLESS SPECIFIED IN WRITING BY THE OWNER.

CONSTRUCTION TIMING

ALL CONSTRUCTION WORK SHALL OCCUR WITHIN THE DESIGNATED IN WATER WORK WINDOW, ANTICIPATED TO OCCUR JULY 15TH THROUGH SEPTEMBER 30TH, 2023.

EROSION CONTROL

CONTRACTOR SHALL BE SOLELY RESPONSIBLE AT OWN EXPENSE FOR PROVIDING AND MAINTAINING ALL NECESSARY EROSION CONTROL FACILITIES TO COMPLY WITH APPLICABLE EROSION CONTROL PERMITS, REGULATIONS, AND TO MAINTAIN CLEAN ACCESS ROUTES.

FISH SALVAGE

PRIOR TO BEGINNING WORK THE SITE SHALL BE ISOLATED AND DE-FISHED. FISH RESCUE TO BE COMPLETED BY EXPERIENCED FISH BIOLOGIST AND COORDINATED WITH OWNER. ADDITIONAL FISH SALVAGE MAY BE REQUIRED IF OVERLAND FLOW ENTERS THE PROJECT AREA DURING A HIGH TIDE.

CULTURAL RESOURCES

IF YOUR WORK BRINGS YOU INTO CONTACT WITH ANY OF THE FOLLOWING CULTURAL RESOURCES:

-NATIVE AMERICAN CULTURAL ARTIFACTS (EXAMPLE: FLAKES, ARROWHEADS, STONE TOOLS, BONE TOOLS, POTTERY, ETC.)

-HISTORIC ERA ARTIFACTS (EXAMPLE: BUILDING FOUNDATIONS, HOMESTEADS, SHIPWRECKS, MINING CAMPS, ETC.)

-HUMAN SKELETAL REMAINS AND BONE FRAGMENTS

YOU MUST IMMEDIATELY DISCONTINUE ALL GROUND-DISTURBING ACTIVITY. DO NOT TOUCH OR MOVE THE OBJECTS AND MAINTAIN THE CONFIDENTIALITY OF THE SITE. FOLLOW THE PROCEDURES LISTED IN THE BPA INADVERTENT DISCOVERY PROCEDURE AND AWAIT FURTHER DIRECTION FROM BPA'S CULTURAL RESOURCES STAFF.

NAME	OFFICE #	CELL #
JENNA PETERSON	(503) 230-3018	N/A
KURT PERKINS	(503) 230-4454	(503) 459-0436
SUNSHINE SCHMIDT	(503) 230-5015	(503) 804-1815

ENVIRONMENTAL PROTECTION

ALL TEMPORARY STAGING AREAS SHALL BE LOCATED AT ELEVATION 12FT (NAVD88) OR HIGHER. ADD FILL TO DESIGNATED STAGING AREAS AS NECESSARY PRIOR TO USE.

THE FOOTPRINT OF FUEL STORAGE AND EQUIPMENT PARKING WITHIN DESIGNATED STAGING AREAS SHALL BE COVERED WITH AN ABRASION RESISTANT OIL ABSORBENT MAT MATERIAL. THE MAT MATERIAL SHALL BE 3-PLY POLYPROPYLENE/POLYETHYLENE OR APPROVED EQUAL.

EQUIPMENT SHALL REMAIN IN DESIGNATED STAGING AREAS AT ANY TIME THE CONTRACTOR IS NOT ON SITE, AND SHALL HAVE A SECOND OIL ABSORBENT MAT INSTALLED UNDER THE CARRIAGE AS A "DIAPER".

ALL REFUELING SHALL TAKE PLACE IN DESIGNATED STAGING AREAS, WITH BOTH GROUND AND "DIAPER" CONTAINMENT IN PLACE.

UTILITIES

THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR HAVING UTILITIES LOCATED PRIOR TO CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL CALL (800-424-5555) FOR UTILITY LOCATE PRIOR TO CONSTRUCTION.

THE CONTRACTOR SHALL IMMEDIATELY CONTACT THE AFFECTED UTILITY SERVICE TO REPORT ANY DAMAGED OR DESTROYED UTILITIES. THE CONTRACTOR SHALL PROVIDE EQUIPMENT AND LABOR TO AID THE AFFECTED UTILITY SERVICE IN REPAIRING DAMAGED OR DESTROYED UTILITIES AT NO ADDITIONAL COST.

CONSTRUCTION STAKING

STAKING OF PROJECT LIMITS, GRADE STAKES, AND ELEVATION CONTROL POINTS BY OTHERS. SOME FIELD ADJUSTMENTS TO THE LINES AND GRADES ARE TO BE EXPECTED.

CONTRACTOR SHALL MEET WITH THE OWNER TO DEFINE AND MARK ACCESS ROUTES AND LIMITS OF DISTURBANCE PRIOR TO MOBILIZATION OF EQUIPMENT OR MATERIALS ONTO THE SITE.

THE CONTRACTOR SHALL REPLACE DAMAGED OR DESTROYED CONSTRUCTION STAKES AT NO ADDITIONAL COST.

EQUIPMENT

EXCAVATORS SHALL BE FITTED WITH NON-TOXIC HYDRAULIC FLUIDS AT NO ADDITIONAL COST.

CONTRACTORS SHALL UTILIZE CONSTRUCTION EQUIPMENT WHICH MINIMIZES IMPACTS TO TIDAL MARSHES - MATS, LOGS, LOW PRESSURE EQUIPMENT OR APPROVED EQUAL.

CONSTRUCTION ACCESS

CONTRACTOR TO NOTIFY G&W PUBLIC PROJECTS DEPARTMENT 30 DAYS PRIOR TO STARTING CONSTRUCTION. G&W FLAGGING SERVICES WILL BE REQUIRED FOR ALL WORK WITHIN G&W RIGHT OF WAY OR ANY WORK THAT HAS A "POTENTIAL TO FOUL".

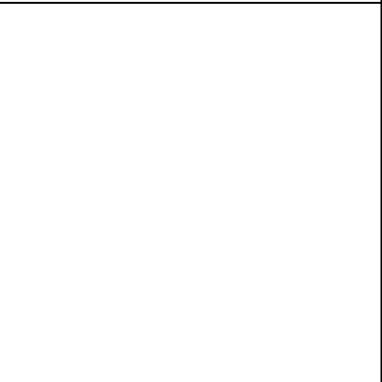
PRIOR TO REVIEW OF ANY SITE ACCESS ON OR ADJACENT TO THE TRACK, PROVIDE EQUIPMENT SPECIFICATIONS TO UTILIZE EQUIPMENT ACCESS ROUTES.

CONTRACTOR SHALL SUBMIT AN ACCESS, STAGING, AND STOCKPILE PLAN TO THE OWNER FOR APPROVAL.

THE CONTRACTOR IS SOLELY RESPONSIBLE FOR PROVIDING ANY REQUIRED TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, SIGNAGE AND FLAGGERS, AND FOR OBTAINING ANY REQUIRED ACCESS PERMITS.

FOR DURATION OF PROJECT, CONTRACTOR SHALL KEEP ALL PRIVATE AND PUBLIC ROADS USED FOR ACCESS FREE OF DEBRIS AND MUD.

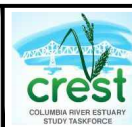
ACCESS WILL INCLUDE TRAVERSING EXISTING UN-UTILIZED RAILROAD GRADE. CONTRACTOR SHALL RETURN THE RAIL LINE AND RAIL CORRIDOR (50FT ON EITHER SIDE OF THE RAILROAD CENTERLINE) TO EXISTING OR BETTER CONDITION, AS APPROVED BY RAIL LINE OWNER.



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**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
 RAILROAD RESTORATION
 AGENCY CREEK - FINAL DESIGN**



GENERAL NOTES (1 OF 2)

SHEET
 2 OF 40

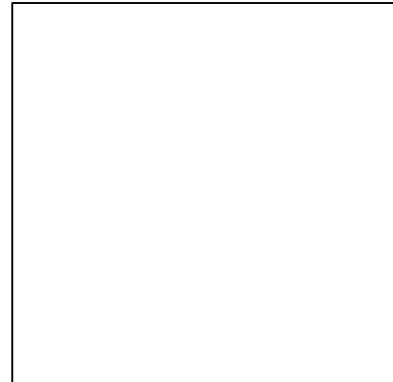
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ABBREVIATIONS

APPROX	APPROXIMATE
AVE	AVERAGE
CMP	CORRUGATED METAL PIPE
CREST	COLUMBIA RIVER ESTUARY STUDY TASKFORCE
CY	CUBIC YARDS
°	DEGREES
DEPT	DEPARTMENT
DIA	DIAMETER
ELEV	ELEVATION
EXIST	EXISTING
FT or '	FT
HORIZ	HORIZONTAL
HWY	HIGHWAY
IN or "	INCHES
INV	INVERT
MAX	MAXIMUM
MHHW	MEAN HIGHER HIGH WATER
MHW	MEAN HIGH WATER
MIN	MINIMUM
MLLW	MEAN LOWER LOW WATER
NOAA	NATIONAL OCEANIC AND ATMOSPHERIC ADMINISTRATION
ODOT	OREGON DEPARTMENT OF TRANSPORTATION
%	PERCENT
RD	ROAD
RMx	RIVER MILE x
ROW	RIGHT OF WAY
STA	STATION
TBD	TO BE DETERMINED
TBM	TEMPORARY BENCHMARK
TYP	TYPICAL
VERT	VERTICAL
WSE	WATER SURFACE ELEVATION
YR	YEAR

QUANTITIES

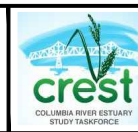
WORK AREA	CUT (CY)	FILL (CY)
LEVEE BREACH A	700	700
LEVEE BREACH B	100	100
LEVEE BREACH C	300	300
RAILROAD PRISM BREACH	300	300
IMPORTED RIPRAP		140
Total	1400	1540



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RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



GENERAL NOTES &
QUANTITIES (2 OF 2)

SHEET
3 OF 40

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HIP GENERAL CONSERVATION MEASURES APPLICABLE TO ALL ACTIONS

THE ACTIVITIES COVERED UNDER THE HIP ARE INTENDED TO PROTECT AND RESTORE FISH AND WILDLIFE HABITAT WITH LONG-TERM BENEFITS TO ESA-LISTED SPECIES. THE FOLLOWING GENERAL CONSERVATION MEASURES (DEVELOPED IN COORDINATION WITH USFWS AND NMFS) WILL BE APPLIED TO ALL ACTIONS OF THIS PROJECT.

PROJECT DESIGN AND SITE PREPARATION.

1. STATE AND FEDERAL PERMITS.

- A. ALL APPLICABLE REGULATORY PERMITS AND OFFICIAL PROJECT AUTHORIZATIONS WILL BE OBTAINED BEFORE PROJECT IMPLEMENTATION.
- B. THESE PERMITS AND AUTHORIZATIONS INCLUDE, BUT ARE NOT LIMITED TO, NATIONAL ENVIRONMENTAL POLICY ACT, NATIONAL HISTORIC PRESERVATION ACT, THE APPROPRIATE STATE AGENCY REMOVAL AND FILL PERMIT, USACE CLEAN WATER ACT (CWA) 404 PERMITS, CWA SECTION 401 WATER QUALITY CERTIFICATIONS, AND FEMA NO-RISE ANALYSES.

2. TIMING OF IN-WATER WORK.

- A. APPROPRIATE STATE (OREGON DEPARTMENT OF FISH AND WILDLIFE (ODFW), WASHINGTON DEPARTMENT OF FISH AND WILDLIFE (WDFW), IDAHO DEPARTMENT OF FISH AND GAME (IDFG), AND MONTANA FISH WILDLIFE AND PARKS (MFWP)) GUIDELINES FOR TIMING OF IN-WATER WORK WINDOWS (IWW) WILL BE FOLLOWED.
- B. CHANGES TO ESTABLISHED WORK WINDOWS WILL BE APPROVED BY REGIONAL STATE BIOLOGISTS AND BPA'S EC LEAD.
- C. BULL TROUT. FOR AREAS WITH DESIGNATED IN-WATER WORK WINDOWS FOR BULL TROUT OR AREAS KNOWN TO HAVE BULL TROUT, PROJECT PROPONENTS WILL CONTACT THE APPROPRIATE USFWS FIELD OFFICE TO INSURE THAT ALL REASONABLE IMPLEMENTATION MEASURES ARE CONSIDERED AND AN APPROPRIATE IN-WATER WORK WINDOW IS BEING USED TO MINIMIZE PROJECT EFFECTS.
- D. LAMPREY. WORKING IN STREAM OR RIVER CHANNELS THAT CONTAIN PACIFIC LAMPREY WILL BE AVOIDED FROM MARCH 1 TO JULY 1 FOR REACHES <5,000 FEET IN ELEVATION AND FROM MARCH 1 TO AUGUST 1 FOR REACHES >5,000 FEET. IF EITHER TIMEFRAME IS INCOMPATIBLE WITH OTHER OBJECTIVES, THE AREA WILL BE SURVEYED FOR NESTS AND LAMPREY PRESENCE, AND AVOIDED IF POSSIBLE. IF LAMPREYS ARE KNOWN TO EXIST, THE PROJECT SPONSOR WILL UTILIZE DEWATERING AND SALVAGE PROCEDURES (SEE FISH SALVAGE AND ELECTROFISHING SECTIONS) TO MINIMIZE ADVERSE EFFECTS.
- E. THE IN-WATER WORK WINDOW WILL BE PROVIDED IN THE CONSTRUCTION PLANS.

3. CONTAMINANTS.

- A. EXCAVATION OF MORE THAN 20 CUBIC YARDS WILL REQUIRE A SITE VISIT AND DOCUMENTED ASSESSMENT FOR POTENTIAL CONTAMINANT SOURCES. THE SITE ASSESSMENT WILL BE STORED WITH PROJECT FILES OR AS AN APPENDIX TO THE BASIS OF DESIGN REPORT.
- B. THE SITE ASSESSMENT WILL SUMMARIZE:
 - 1. THE SITE VISIT, CONDITION OF THE PROPERTY, AND IDENTIFICATION OF ANY AREAS USED FOR VARIOUS INDUSTRIAL PROCESSES;
 - 2. AVAILABLE RECORDS, SUCH AS FORMER SITE USE, BUILDING PLANS, AND RECORDS OF ANY PRIOR CONTAMINATION EVENTS;
 - 3. INTERVIEWS WITH KNOWLEDGEABLE PEOPLE, SUCH AS SITE OWNERS, OPERATORS, OCCUPANTS, NEIGHBORS, OR LOCAL GOVERNMENT OFFICIALS; AND
 - 4. THE TYPE, QUANTITY, AND EXTENT OF ANY POTENTIAL CONTAMINATION SOURCES.

4. SITE LAYOUT AND FLAGGING.

- A. CONSTRUCTION AREAS TO BE CLEARLY FLAGGED PRIOR TO CONSTRUCTION.
- B. AREAS TO BE FLAGGED WILL INCLUDE:
 - 1. SENSITIVE RESOURCE AREAS, SUCH AS AREAS BELOW ORDINARY HIGH WATER, SPAWNING AREAS, SPRINGS, AND WETLANDS;
 - 2. EQUIPMENT ENTRY AND EXIT POINTS;
 - 3. ROAD AND STREAM CROSSING ALIGNMENTS;
 - 4. STAGING, STORAGE, AND STOCKPILE AREAS; AND
 - 5. NO-SPRAY AREAS AND BUFFERS.

5. TEMPORARY ACCESS ROADS AND PATHS.

- A. EXISTING ACCESS ROADS AND PATHS WILL BE PREFERENTIALLY USED WHENEVER REASONABLE, AND THE NUMBER AND LENGTH OF TEMPORARY ACCESS ROADS AND PATHS THROUGH RIPARIAN AREAS AND FLOODPLAINS WILL BE MINIMIZED.
- B. VEHICLE USE AND HUMAN ACTIVITIES, INCLUDING WALKING, IN AREAS OCCUPIED BY TERRESTRIAL ESA-LISTED SPECIES WILL BE MINIMIZED.
- C. TEMPORARY ACCESS ROADS AND PATHS WILL NOT BE BUILT ON SLOPES WHERE GRADE, SOIL, OR OTHER FEATURES SUGGEST A LIKELIHOOD OF EXCESSIVE EROSION OR FAILURE. IF SLOPES ARE STEEPER THAN 30%, THEN THE ROAD WILL BE DESIGNED BY A CIVIL ENGINEER WITH EXPERIENCE IN STEEP ROAD DESIGN.
- D. THE REMOVAL OF RIPARIAN VEGETATION DURING CONSTRUCTION OF TEMPORARY ACCESS ROADS WILL BE MINIMIZED. WHEN TEMPORARY VEGETATION REMOVAL IS REQUIRED, VEGETATION WILL BE CUT AT GROUND LEVEL (NOT GRUBBED).
- E. AT PROJECT COMPLETION, ALL TEMPORARY ACCESS ROADS AND PATHS WILL BE OBLITERATED, AND THE SOIL WILL BE STABILIZED AND REVEGETATED. ROAD AND PATH OBLITERATION REFERS TO THE MOST COMPREHENSIVE DEGREE OF DECOMMISSIONING AND INVOLVES DECOMPACTING THE SURFACE AND DITCH, PULLING THE FILL MATERIAL ONTO THE RUNNING SURFACE, AND RESHAPING TO MATCH THE ORIGINAL CONTOUR.
- F. HELICOPTER FLIGHT PATTERNS WILL BE ESTABLISHED IN ADVANCE AND LOCATED TO AVOID TERRESTRIAL ESA-LISTED SPECIES AND THEIR OCCUPIED HABITAT DURING SENSITIVE LIFE STAGES.

6. TEMPORARY STREAM CROSSINGS.

- A. EXISTING STREAM CROSSINGS OR BEDROCK WILL BE PREFERENTIALLY USED WHENEVER REASONABLE, AND THE NUMBER OF TEMPORARY STREAM CROSSINGS WILL BE MINIMIZED.
- B. TEMPORARY BRIDGES AND CULVERTS WILL BE INSTALLED TO ALLOW FOR EQUIPMENT AND VEHICLE CROSSING OVER PERENNIAL STREAMS DURING CONSTRUCTION. TREATED WOOD SHALL NOT BE USED ON TEMPORARY BRIDGE CROSSINGS OR IN LOCATIONS IN CONTACT WITH OR DIRECTLY OVER WATER.
- C. FOR PROJECTS THAT REQUIRE EQUIPMENT AND VEHICLES TO CROSS IN THE WET:
 - 1. THE LOCATION AND NUMBER OF ALL WET CROSSINGS SHALL BE APPROVED BY THE BPA EC LEAD AND DOCUMENTED IN THE CONSTRUCTION PLANS;
 - 2. VEHICLES AND MACHINERY SHALL CROSS STREAMS AT RIGHT ANGLES TO THE MAIN CHANNEL WHENEVER POSSIBLE;
 - 3. NO STREAM CROSSINGS WILL OCCUR 300 FEET UPSTREAM OR 100 FEET DOWNSTREAM OF AN EXISTING REDD OR SPAWNING FISH; AND
 - 4. AFTER PROJECT COMPLETION, TEMPORARY STREAM CROSSINGS WILL BE OBLITERATED AND BANKS RESTORED.

7. STAGING, STORAGE, AND STOCKPILE AREAS.

- A. STAGING AREAS (USED FOR CONSTRUCTION EQUIPMENT STORAGE, VEHICLE STORAGE, FUELING, SERVICING, AND HAZARDOUS MATERIAL STORAGE) ~~WILL BE 150 FEET OR MORE FROM ANY NATURAL WATER BODY OR WETLAND. STAGING AREAS CLOSER THAN 150 FEET WILL BE APPROVED BY THE EC LEAD.~~
- B. NATURAL MATERIALS USED FOR IMPLEMENTATION OF AQUATIC RESTORATION, SUCH AS LARGE WOOD, GRAVEL, AND BOULDERS, MAY BE STAGED WITHIN 150 FEET IF CLEARLY INDICATED IN THE PLANS THAT AREA IS FOR NATURAL MATERIALS ONLY.
- C. ANY LARGE WOOD, TOPSOIL, AND NATIVE CHANNEL MATERIAL DISPLACED BY CONSTRUCTION WILL BE STOCKPILED FOR USE DURING SITE RESTORATION AT A SPECIFICALLY IDENTIFIED AND FLAGGED AREA.
- D. ANY MATERIAL NOT USED IN RESTORATION, AND NOT NATIVE TO THE FLOODPLAIN, WILL BE DISPOSED OF OUTSIDE THE 100-YEAR FLOODPLAIN.

8. EQUIPMENT.

- A. MECHANIZED EQUIPMENT AND VEHICLES WILL BE SELECTED, OPERATED, AND MAINTAINED IN A MANNER THAT MINIMIZES ADVERSE EFFECTS ON THE ENVIRONMENT (E.G., MINIMALLY-SIZED, LOW PRESSURE TIRES; MINIMAL HARD-TURN PATHS FOR TRACKED VEHICLES; TEMPORARY MATS OR PLATES WITHIN WET AREAS OR ON SENSITIVE SOILS).
- B. EQUIPMENT WILL BE STORED, FUELED, AND MAINTAINED IN AN CLEARLY IDENTIFIED STAGING AREA THAT MEETS STAGING AREA CONSERVATION MEASURES.

- C. EQUIPMENT WILL BE REFUELED IN A VEHICLE STAGING AREA OR IN AN ISOLATED HARD ZONE, SUCH AS A PAVED PARKING LOT OR ADJACENT, ESTABLISHED ROAD (THIS MEASURE APPLIES ONLY TO GAS-POWERED EQUIPMENT WITH TANKS LARGER THAN 5 GALLONS).
- D. BIODEGRADABLE LUBRICANTS AND FLUIDS WILL BE USED ON EQUIPMENT OPERATING IN AND ADJACENT TO THE STREAM CHANNEL AND LIVE WATER.
- E. EQUIPMENT WILL BE INSPECTED DAILY FOR FLUID LEAKS BEFORE LEAVING THE VEHICLE STAGING AREA FOR OPERATION WITHIN 150 FEET OF ANY NATURAL WATER BODY OR WETLAND.
- F. EQUIPMENT WILL BE THOROUGHLY CLEANED BEFORE OPERATION BELOW ORDINARY HIGH WATER, AND AS OFTEN AS NECESSARY DURING OPERATION, TO REMAIN GREASE FREE.

9. EROSION CONTROL.

- A. TEMPORARY EROSION CONTROL MEASURES INCLUDE:
 - 1. TEMPORARY EROSION CONTROLS WILL BE IN PLACE BEFORE ANY SIGNIFICANT ALTERATION OF THE ACTION SITE AND APPROPRIATELY INSTALLED DOWNSLOPE OF PROJECT ACTIVITY WITHIN THE RIPARIAN BUFFER AREA UNTIL SITE REHABILITATION IS COMPLETE;
 - 2. IF THERE IS A POTENTIAL FOR ERODED SEDIMENT TO ENTER THE STREAM, SEDIMENT BARRIERS WILL BE INSTALLED AND MAINTAINED FOR THE DURATION OF PROJECT IMPLEMENTATION;
 - 3. TEMPORARY EROSION CONTROL MEASURES MAY INCLUDE SEDGE MATS, FIBER WATTLES, SILT FENCES, JUTE MATTING, WOOD FIBER MULCH AND SOIL BINDER, OR GEOTEXTILES AND GEOSYNTHETIC FABRIC;
 - 4. SOIL STABILIZATION UTILIZING WOOD FIBER MULCH AND TACKIFIER (HYDRO-APPLIED) MAY BE USED TO REDUCE EROSION OF BARE SOIL IF THE MATERIALS ARE NOXIOUS WEED FREE AND NONTOXIC TO AQUATIC AND TERRESTRIAL ANIMALS, SOIL MICROORGANISMS, AND VEGETATION;
 - 5. SEDIMENT WILL BE REMOVED FROM EROSION CONTROLS ONCE IT HAS REACHED 1/3 OF THE EXPOSED HEIGHT OF THE CONTROL; AND
 - 6. ONCE THE SITE IS STABILIZED AFTER CONSTRUCTION, TEMPORARY EROSION CONTROL MEASURES WILL BE REMOVED.
- B. EMERGENCY EROSION CONTROLS. THE FOLLOWING MATERIALS FOR EMERGENCY EROSION CONTROL WILL BE AVAILABLE AT THE WORK SITE:
 - 1. A SUPPLY OF SEDIMENT CONTROL MATERIALS; AND
 - 2. AN OIL-ABSORBING FLOATING BOOM WHENEVER SURFACE WATER IS PRESENT.

10. DUST ABATEMENT.

- A. THE PROJECT SPONSOR WILL DETERMINE THE APPROPRIATE DUST CONTROL MEASURES BY CONSIDERING SOIL TYPE, EQUIPMENT USAGE, PREVAILING WIND DIRECTION, AND THE EFFECTS CAUSED BY OTHER EROSION AND SEDIMENT CONTROL MEASURES.
- B. WORK WILL BE SEQUENCED AND SCHEDULED TO REDUCE EXPOSED BARE SOIL SUBJECT TO WIND EROSION.
- C. DUST-ABATEMENT ADDITIVES AND STABILIZATION CHEMICALS (TYPICALLY MAGNESIUM CHLORIDE, CALCIUM CHLORIDE SALTS, OR LIGNINSULFONATE) WILL NOT BE APPLIED WITHIN 25 FEET OF WATER OR A STREAM CHANNEL AND WILL BE APPLIED SO AS TO MINIMIZE THE LIKELIHOOD THAT THEY WILL ENTER STREAMS. APPLICATIONS OF LIGNINSULFONATE WILL BE LIMITED TO A MAXIMUM RATE OF 0.5 GALLONS PER SQUARE YARD OF ROAD SURFACE, ASSUMING MIXED 50:50 WITH WATER.
- D. APPLICATION OF DUST ABATEMENT CHEMICALS WILL BE AVOIDED DURING OR JUST BEFORE WET WEATHER, AND AT STREAM CROSSINGS OR OTHER AREAS THAT COULD RESULT IN UNFILTERED DELIVERY OF THE DUST ABATEMENT MATERIALS TO A WATERBODY (TYPICALLY THESE WOULD BE AREAS WITHIN 25 FEET OF A WATERBODY OR STREAM CHANNEL; DISTANCES MAY BE GREATER WHERE VEGETATION IS SPARSE OR SLOPES ARE STEEP).
- E. SPILL CONTAINMENT EQUIPMENT WILL BE AVAILABLE DURING APPLICATION OF DUST ABATEMENT CHEMICALS.
- F. PETROLEUM-BASED PRODUCTS WILL NOT BE USED FOR DUST ABATEMENT.

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**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**



**HIP GENERAL CONSERVATION
MEASURES (1 OF 3)**

PROJECT DESIGN AND SITE PREPARATION (CONTINUED).

11. SPILL PREVENTION, CONTROL, AND COUNTER MEASURES.

- A. A DESCRIPTION OF HAZARDOUS MATERIALS THAT WILL BE USED, INCLUDING INVENTORY, STORAGE, AND HANDLING PROCEDURES WILL BE AVAILABLE ON-SITE.
- B. WRITTEN PROCEDURES FOR NOTIFYING ENVIRONMENTAL RESPONSE AGENCIES WILL BE POSTED AT THE WORK SITE.
- C. SPILL CONTAINMENT KITS (INCLUDING INSTRUCTIONS FOR CLEANUP AND DISPOSAL) ADEQUATE FOR THE TYPES AND QUANTITY OF HAZARDOUS MATERIALS USED AT THE SITE WILL BE AVAILABLE AT THE WORK SITE.
- D. WORKERS WILL BE TRAINED IN SPILL CONTAINMENT PROCEDURES AND WILL BE INFORMED OF THE LOCATION OF SPILL CONTAINMENT KITS.
- E. ANY WASTE LIQUIDS GENERATED AT THE STAGING AREAS WILL BE TEMPORARILY STORED UNDER AN IMPERVIOUS COVER, SUCH AS A TARPULIN, UNTIL THEY CAN BE PROPERLY TRANSPORTED TO AND DISPOSED OF AT A FACILITY THAT IS APPROVED FOR RECEIPT OF HAZARDOUS MATERIALS.
- F. PUMPS USED ADJACENT TO WATER SHALL USE SPILL CONTAINMENT SYSTEMS.

12. INVASIVE SPECIES CONTROL.

- A. PRIOR TO ENTERING THE SITE, ALL VEHICLES AND EQUIPMENT WILL BE POWER WASHED, ALLOWED TO FULLY DRY, AND INSPECTED TO MAKE SURE NO PLANTS, SOIL, OR OTHER ORGANIC MATERIAL ADHERES TO THE SURFACE.
- B. WATERCRAFT, WADERS, BOOTS, AND ANY OTHER GEAR TO BE USED IN OR NEAR WATER WILL BE INSPECTED FOR AQUATIC INVASIVE SPECIES.
- C. WADING BOOTS WITH FELT SOLES ARE NOT TO BE USED DUE TO THEIR PROPENSITY FOR AIDING IN THE TRANSFER OF INVASIVE SPECIES UNLESS DECONTAMINATION PROCEDURES HAVE BEEN APPROVED BY THE EC LEAD.

WORK AREA ISOLATION AND FISH SALVAGE.

1. WORK AREA ISOLATION.

- A. ANY WORK AREA WITHIN THE WETTED CHANNEL WILL BE ISOLATED FROM THE ACTIVE STREAM WHENEVER ESA-LISTED FISH ARE REASONABLY CERTAIN TO BE PRESENT, OR IF THE WORK AREA IS LESS THAN 300-FEET UPSTREAM FROM KNOWN SPAWNING HABITATS.
- B. WORK AREA ISOLATION AND FISH SALVAGE ACTIVITIES WILL COMPLY WITH THE IN-WATER WORK WINDOW.
- C. DESIGN PLANS WILL INCLUDE ALL ISOLATION ELEMENTS AND AREAS (COFFER DAMS, PUMPS, DISCHARGE AREAS, FISH SCREENS, FISH RELEASE AREAS, ETC.).
- D. WORK AREA ISOLATION AND FISH CAPTURE ACTIVITIES WILL OCCUR DURING PERIODS OF THE COOLEST AIR AND WATER TEMPERATURES POSSIBLE, NORMALLY EARLY IN THE MORNING VERSUS LATE IN THE DAY, AND DURING CONDITIONS APPROPRIATE TO MINIMIZE STRESS AND DEATH OF SPECIES PRESENT.

2. FISH SALVAGE.

- A. MONITORING AND RECORDING WILL TAKE PLACE FOR DURATION OF SALVAGE. THE SALVAGE REPORT WILL BE COMMUNICATED TO AGENCIES VIA THE PROJECT COMPLETION FORM (PCF).
- B. SALVAGE ACTIVITIES SHOULD TAKE PLACE DURING CONDITIONS TO MINIMIZE STRESS TO FISH SPECIES, TYPICALLY PERIODS OF THE COOLEST AIR AND WATER TEMPERATURES WHICH OCCUR IN THE MORNING VERSUS LATE IN THE DAY.
- C. SALVAGE OPERATIONS WILL FOLLOW THE ORDERING, METHODS, AND CONSERVATION MEASURES SPECIFIED BELOW:
 - 1. SLOWLY REDUCE WATER FROM THE WORK AREA TO ALLOW SOME FISH TO LEAVE VOLITIONALLY.
 - 2. BLOCK NETS WILL BE INSTALLED AT UPSTREAM AND DOWNSTREAM LOCATIONS AND MAINTAINED IN A SECURED POSITION TO EXCLUDE FISH FROM ENTERING THE PROJECT AREA.
 - 3. BLOCK NETS WILL BE SECURED TO THE STREAM CHANNEL BED AND BANKS UNTIL FISH CAPTURE AND TRANSPORT ACTIVITIES ARE COMPLETE. BLOCK NETS MAY BE LEFT IN PLACE FOR THE DURATION OF THE PROJECT TO EXCLUDE FISH AS LONG AS PASSAGE REQUIREMENTS ARE MET.
 - 4. NETS WILL BE MONITORED HOURLY DURING IN-STREAM DISTURBANCE.

- 5. IF BLOCK NETS REMAIN IN PLACE MORE THAN ONE DAY, THE NETS WILL BE MONITORED AT LEAST DAILY TO ENSURE THEY ARE SECURED AND FREE OF ORGANIC ACCUMULATION. IF BULL TROUT ARE PRESENT, NETS ARE TO BE CHECKED EVERY 4 HOURS FOR FISH IMPINGEMENT.
- 6. CAPTURE FISH THROUGH SEINING AND RELOCATE TO STREAMS.
- 7. WHILE DEWATERING, ANY REMAINING FISH WILL BE COLLECTED BY HAND OR DIP NETS.
- 8. SEINES WITH A MESH SIZE TO ENSURE CAPTURE OF THE RESIDING ESA-LISTED FISH WILL BE USED.
- 9. MINNOW TRAPS WILL BE LEFT IN PLACE OVERNIGHT AND USED IN CONJUNCTION WITH SEINING.
- 10. ELECTROFISH TO CAPTURE AND RELOCATED FISH NOT CAUGHT DURING SEINING PER ELECTROFISH CONSERVATION MEASURES.
- 11. CONTINUE TO SLOWLY DEWATER STREAM REACH.
- 12. COLLECT ANY REMAINING FISH IN COLD-WATER BUCKETS AND RELOCATED TO THE STREAM.
- 13. LIMIT THE TIME FISH ARE IN A TRANSPORT BUCKET.
- 14. MINIMIZE PREDATION BY TRANSPORTING COMPARABLE SIZES IN BUCKETS.
- 15. BUCKET WATER TO BE CHANGED EVERY 15 MINUTES OR AERATED.
- 16. BUCKETS WILL BE KEPT IN SHADED AREAS OR COVERED.
- 17. DEAD FISH WILL NOT BE STORED IN TRANSPORT BUCKETS, BUT WILL BE LEFT ON THE STREAM BANK TO AVOID MORTALITY COUNTING ERRORS.
- D. SALVAGE GUIDELINES FOR BULL TROUT, LAMPREY, MUSSELS, AND NATIVE FISH.
 - 1. CONDUCT SITE SURVEY TO ESTIMATE SALVAGE NUMBERS.
 - 2. PRE-SELECT SITE(S) FOR RELEASE AND/OR MUSSEL BED RELOCATION.
 - 3. SALVAGE OF BULL TROUT WILL NOT TAKE PLACE WHEN WATER TEMPERATURES EXCEED 15 DEGREES CELSIUS.
 - 4. IF DRAWDOWN LESS THAN 48 HOURS, SALVAGE OF LAMPREY AND MUSSELS MAY NOT BE NECESSARY IF TEMPERATURES SUPPORT SURVIVAL IN SEDIMENTS.
 - 5. SALVAGE MUSSELS BY HAND, LOCATING BY SNORKELING OR WADING.
 - 6. SALVAGE LAMPREY BY ELECTROFISHING (SEE ELECTROFISHING FOR LARVAL LAMPREY SETTINGS AND LARVAL LAMPREY DRY SHOCKING SETTINGS).
 - 7. SALVAGE BONY FISH AFTER LAMPREY WITH NETS OR ELECTROFISHING (SEE ELECTROFISHING FOR APPROPRIATE SETTINGS).
 - 8. REGULARLY INSPECT DEWATERED SITE SINCE LAMPREY LIKELY TO EMERGE AFTER DEWATERING AND MUSSELS MAY BECOME VISIBLE.
 - 9. MUSSELS MAY BE TRANSFERRED IN COOLERS.
 - 10. MUSSELS WILL BE PLACED INDIVIDUALLY TO ENSURE ABILITY TO BURROW INTO NEW HABITAT.

3. ELECTROFISHING.

- A. INITIAL SITE SURVEY AND INITIAL SETTINGS.
 - 1. IDENTIFY SPAWNING ADULTS AND ACTIVE REDDS TO AVOID.
 - 2. RECORD WATER TEMPERATURE. ELECTROFISHING WILL NOT OCCUR WHEN WATER TEMPERATURES ARE ABOVE 18 DEGREES CELSIUS.
 - 3. IF POSSIBLE, A BLOCK NET WILL BE PLACED DOWNSTREAM AND CHECKED REGULARLY TO CAPTURE STUNNED FISH THAT DRIFT DOWNSTREAM.
 - 4. INITIAL SETTINGS WILL BE 100 VOLTS, PULSE WIDTH OF 500 MICRO SECONDS, AND PULSE RATE OF 30 HERTZ.
 - 5. RECORDS FOR CONDUCTIVITY, WATER TEMPERATURE, AIR TEMPERATURE, ELECTROFISHING SETTINGS, ELECTROFISHER MODEL, ELECTROFISHER CALIBRATION, FISH CONDITIONS, FISH MORTALITIES, AND TOTAL CAPTURE RATES WILL BE INCLUDED IN THE SALVAGE LOG BOOK.

B. ELECTROFISHING TECHNIQUE.

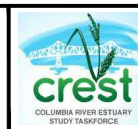
- 1. SAMPLING WILL BEGIN USING STRAIGHT DC. POWER WILL REMAIN ON UNTIL THE FISH IS NETTED WHEN USING STRAIGHT DC. GRADUALLY INCREASE VOLTAGE WHILE REMAINING BELOW MAXIMUM LEVELS.
- 2. MAXIMUM VOLTAGE WILL BE 1100 VOLTS WHEN CONDUCTIVITY IS <100 MILLISECONDS, 800 VOLTS WHEN CONDUCTIVITY IS BETWEEN 100 AND 300 MILLISECONDS, AND 400 VOLTS WHEN CONDUCTIVITY IS >300 MILLISECONDS.
- 3. IF FISH CAPTURE IS NOT SUCCESSFUL USING STRAIGHT DC, THE ELECTROFISHER WILL BE SET TO INITIAL VOLTAGE FOR PDC. VOLTAGE, PULSE WIDTH, AND PULSE FREQUENCY WILL BE GRADUALLY INCREASED WITHIN MAXIMUM VALUES UNTIL CAPTURE IS SUCCESSFUL.
- 4. MAXIMUM PULSE WIDTH IS 5 MILLISECONDS. MAXIMUM PULSE RATE IS 70 HERTZ
- 5. ELECTROFISHING WILL NOT OCCUR IN ONE AREA FOR AN EXTENDED PERIOD.
- 6. THE ANODE WILL NOT INTENTIONALLY COME INTO CONTACT WITH FISH. THE ZONE FOR POTENTIAL INJURY OF 0.5 M FROM THE ANODE WILL BE AVOIDED.
- 7. SETTINGS WILL BE LOWERED IN SHALLOWER WATER SINCE VOLTAGE GRADIENTS LIKELY TO INCREASE.
- 8. ELECTROFISHING WILL NOT OCCUR IN TURBID WATER WHERE VISIBILITY IS POOR (I.E. UNABLE TO SEE THE BED OF THE STREAM).
- 9. OPERATIONS WILL IMMEDIATELY STOP IF MORTALITY OR OBVIOUS FISH INJURY IS OBSERVED. ELECTROFISHING SETTINGS WILL BE REEVALUATED.
- C. SAMPLE PROCESSING.
 - 1. FISH SHALL BE SORTED BY SIZE TO AVOID PREDATION DURING CONTAINMENT.
 - 2. SAMPLERS WILL REGULARLY CHECK CONDITIONS OF FISH HOLDING CONTAINERS, AIR PUMPS, WATER TRANSFERS, ETC.
 - 3. FISH WILL BE OBSERVED FOR GENERAL CONDITIONS AND INJURIES
 - 4. EACH FISH WILL BE COMPLETELY REVIVED BEFORE RELEASE. ESA-LISTED SPECIES WILL BE PRIORITIZED FOR SUCCESSFUL RELEASE.
- D. BULL TROUT ELECTROFISHING.
 - 1. ELECTROFISHING FOR BULL TROUT WILL ONLY OCCUR FROM MAY 1 TO JULY 31. NO ELECTROFISHING WILL OCCUR IN ANY BULL TROUT OCCUPIED HABITAT AFTER AUGUST 15. IN FMO HABITATS ELECTROFISHING MAY OCCUR ANY TIME.
 - 2. ELECTROFISHING OF BULL TROUT WILL NOT OCCUR WHEN WATER TEMPERATURES EXCEED 15 DEGREES CELSIUS.

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**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**



**HIP GENERAL CONSERVATION
MEASURES (2 OF 3)**

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WORK AREA ISOLATION AND FISH SALVAGE (CONTINUED).

E. LARVAL LAMPREY ELECTROFISHING.

1. PERMISSION FROM EC LEAD WILL BE OBTAINED IF LARVAL LAMPREY ELECTROFISHER IS NOT ONE OF FOLLOWING PRE-APPROVED MODELS: ABP-2 "WISCONSIN", SMITH-ROOT LR-24, OR SMITH-ROOT APEX BACKPACK.
2. LARVAL LAMPREY SAMPLING WILL INCORPORATE 2-STAGE METHOD: "TICKLE" AND "STUN".
3. FIRST STAGE: USE 125 VOLT DC WITH A 25 PERCENT DUTY CYCLE APPLIED AT A SLOW RATE OF 3 PULSES PER SECOND. IF TEMPERATURES ARE BELOW 10 DEGREES CELSIUS, VOLTAGE MAY BE INCREASED GRADUALLY (NOT TO EXCEED 200 VOLTS). BURSTED PULSES (THREE SLOW AND ONE SKIPPED) RECOMMENDED TO INCREASE EMERGENCE.
4. SECOND STAGE (OPTIONAL FOR EXPERIENCED NETTERS): IMMEDIATELY AFTER LAMPREY EMERGE, USE A FAST PULSE SETTING OF 30 PULSES PER SECOND.
5. USE DIP NETS FOR VISIBLE LAMPREY. SIENES AND FINE MESH NET SWEEPS MAY BE USED IN POOR VISIBILITY.
6. SAMPLING WILL OCCUR SLOWLY (>60 SECONDS PER METER) STARTING AT UPSTREAM AND WORKING DOWNSTREAM.
7. MULTIPLE SWEEPS TO OCCUR WITH 15 MINUTES BETWEEN SWEEPS.
8. POST-DRAWDOWN "DRY-SHOCKING" WILL BE APPLIED IF LARVAL LAMPREY CONTINUE TO EMERGE. ANODES TO BE PLACED ONE METER APART TO SAMPLE ONE SQUARE METER AT A TIME FOR AT LEAST 60 SECONDS. FOR TEMPERATURES LESS THAN 10 DEGREES CELSIUS, MAXIMUM VOLTAGE MAY BE GRADUALLY INCREASED TO 400 VOLTS (DRY-SHOCKING ONLY).

4. DEWATERING.

- A. DEWATERING WILL OCCUR AT A RATE SLOW ENOUGH TO ALLOW SPECIES TO NATURALLY MIGRATE OUT OF THE WORK AREA.
- B. WHERE A GRAVITY FEED DIVERSION IS NOT POSSIBLE, A PUMP MAY BE USED. PUMPS WILL BE INSTALLED TO AVOID REPETITIVE DEWATERING AND REWATERING.
- C. WHEN FISH ARE PRESENT, PUMPS WILL BE SCREENED IN ACCORDANCE WITH NMFS FISH SCREEN CRITERIA. NMFS ENGINEERING REVIEW AND APPROVAL WILL BE OBTAINED FOR PUMPS EXCEEDING 3 CUBIC FEET PER SECOND.
- D. DISSIPATION OF FLOW ENERGY AT THE BYPASS OUTFLOW WILL BE PROVIDED TO PREVENT DAMAGE TO THE STREAM CHANNEL AND RIPARIAN VEGETATION.
- E. SEEPAGE WATER WILL BE PUMPED TO A TEMPORARY STORAGE AND TREATMENT SITE OF INTO UPLAND AREAS TO ALLOW WATER TO PERCOLATE THROUGH SOIL AND VEGETATION PRIOR TO REENTERING THE STREAM CHANNEL.

CONSTRUCTION AND POST CONSTRUCTION CONSERVATION MEASURES.

1. FISH PASSAGE.

- A. FISH PASSAGE WILL BE PROVIDED FOR ADULT AND JUVENILE FISH LIKELY TO BE PRESENT DURING CONSTRUCTION UNLESS PASSAGE DID NOT EXIST BEFORE CONSTRUCTION, THE STREAM IS NATURALLY IMPASSABLE, OR PASSAGE WILL NEGATIVELY IMPACT ESA-LISTED SPECIES OR THEIR HABITAT.
- B. FISH PASSAGE ALTERNATIVES WILL BE APPROVED BY THE BPA EC LEAD UNDER ADVISEMENT BY THE NMFS HABITAT BIOLOGIST.

2. CONSTRUCTION AND DISCHARGE WATER.

- A. SURFACE WATER MAY BE DIVERTED TO MEET CONSTRUCTION NEEDS ONLY IF DEVELOPED SOURCES ARE UNAVAILABLE OR INADEQUATE.
- B. DIVERSIONS WILL NOT EXCEED 10% OF THE AVAILABLE FLOW.
- C. CONSTRUCTION DISCHARGE WATER WILL BE COLLECTED AND TREATED TO REMOVE DEBRIS, NUTRIENTS, SEDIMENT, PETROLEUM HYDROCARBONS, METALS, AND OTHER POLLUTANTS.

3. TIME AND EXTENT OF DISTURBANCE.

- A. EARTHWORK REQUIRING IN-STREAM MECHANIZED EQUIPMENT (INCLUDING DRILLING, EXCAVATION, DREDGING, FILLING, AND COMPACTING) WILL BE COMPLETED AS QUICKLY AS POSSIBLE.
- B. MECHANIZED EQUIPMENT WILL WORK FROM TOP OF BANK UNLESS WORK FROM ANOTHER LOCATION WILL RESULT IN LESS HABITAT DISTURBANCE (TURBIDITY, VEGETATION DISTURBANCE, ETC.).

4. CESSATION OF WORK.

- A. PROJECT OPERATIONS WILL CEASE WHEN HIGH FLOW CONDITIONS MAY RESULT IN INUNDATION OF THE PROJECT AREA (FLOOD EFFORTS TO DECREASE DAMAGES TO NATURAL RESOURCES PERMITTED).

- B. WATER QUALITY LEVELS EXCEEDED. SEE CWA SECTION 401 WATER QUALITY CERTIFICATION AND TURBIDITY MEASURES.

5. SITE RESTORATION.

- A. DISTURBED AREAS, STREAM BANKS, SOILS, AND VEGETATION WILL BE CLEANED UP AND RESTORED TO IMPROVED OR PRE-PROJECT CONDITIONS.
- B. PROJECT-RELATED WASTE WILL BE REMOVED.
- C. TEMPORARY ACCESS ROADS AND STAGING WILL BE DECOMPACTED AND RESTORED. SOILS WILL BE LOOSENEED IF NEEDED FOR REVEGETATION OR WATER INFILTRATION.
- D. THE PROJECT SPONSOR WILL RETAIN THE RIGHT OF REASONABLE ACCESS TO THE SITE TO MONITOR AND MAINTAIN THE SITE OVER THE LIFE OF THE PROJECT.

6. REVEGETATION.

- A. PLANTING AND SEEDING WILL OCCUR PRIOR TO OR AT THE BEGINNING OF THE FIRST GROWING SEASON AFTER CONSTRUCTION.
- B. A MIX OF NATIVE SPECIES (INVASIVE SPECIES NOT ALLOWED) APPROPRIATE TO THE SITE WILL BE USED TO REESTABLISH VEGETATION, PROVIDE SHADE, AND REDUCE EROSION. REESTABLISHED VEGETATION SHOULD BE AT LEAST 70% OF PRE-PROJECT CONDITIONS WITHIN THREE YEARS.
- C. VEGETATION SUCH AS WILLOWS, SEDGES, OR RUSH MATS WILL BE SALVAGED FROM DISTURBED OR ABANDONED AREAS TO BE REPLANTED.
- D. SHORT-TERM STABILIZATION MEASURE MAY INCLUDE THE USE OF NON-NATIVE STERILE SEED MIX (WHEN NATIVE NOT AVAILABLE), WEED-FREE CERTIFIED STRAW, OR OTHER SIMILAR TECHNIQUES.
- E. SURFACE FERTILIZER WILL NOT BE APPLIED WITHIN 50 FEET OF ANY STREAM, WATE BODY, OR WETLAND.
- F. FENCING WILL BE INSTALLED AS NECESSARY TO PREVENT ACCESS TO REVEGETATED SITES BY LIVESTOCK OR UNAUTHORIZED PERSONS.
- G. INVASIVE PLANTS WILL BE REMOVED OR CONTROLLED UNTIL NATIVE PLANT SPECIES ARE WELL ESTABLISHED (TYPICALLY THREE YEARS POST-CONSTRUCTION).

7. SITE ACCESS AND IMPLEMENTATION MONITORING.

- A. THE PROJECT SPONSOR WILL PROVIDE CONSTRUCTION MONITORING DURING IMPLEMENTATION TO ENSURE ALL CONSERVATION MEASURES ARE ADEQUATELY FOLLOWED, EFFECTS TO LISTED SPECIES ARE NOT GREATER THAN PREDICTED, AND INCIDENTAL TAKE LIMITATIONS ARE NOT EXCEEDED.
- B. THE PROJECT SPONSOR OR DESIGNATED REPRESENTATIVE WILL SUBMIT THE PROJECT COMPLETION FORM (PCF) WITHIN 30 DAYS OF PROJECT COMPLETION.

8. CWA SECTION 401 WATER QUALITY CERTIFICATION.

- A. THE PROJECT SPONSOR OR DESIGNATED REPRESENTATIVE WILL COMPLETE AND RECORD WATER QUALITY OBSERVATIONS (SEE TURBIDITY MONITORING) TO ENSURE IN-WATER WORK IS NOT DEGRADING WATER QUALITY.
- B. DURING CONSTRUCTION, WATER QUALITY PROVISIONS PROVIDED BY THE OREGON DEPARTMENT OF ENVIRONMENTAL QUALITY, WASHINGTON DEPARTMENT OF ECOLOGY, IDAHO DEPARTMENT OF ENVIRONMENTAL QUALITY WILL BE FOLLOWED.

STAGED REWATERING PLAN.

- A. WHEN REINTRODUCING WATER TO DEWATERED AREAS AND NEWLY CONSTRUCTED CHANNELS, A STAGED REWATERING PLAN WILL BE APPLIED.
- B. THE FOLLOWING WILL BE APPLIED TO ALL REWATERING EFFORTS. COMPLEX REWATERING EFFORTS MAY REQUIRE ADDITIONAL NOTES OR A DEDICATED SHEET IN THE CONSTRUCTION DETAILS.

1. TURBIDITY MONITORING PROTOCOL WILL BE APPLIED TO REWATERING EFFORTS.
2. PRE-WASH THE AREA BEFORE REWATERING. TURBID WASH WATER WILL BE DETAINED AND PUMPED TO THE FLOODPLAIN OR SEDIMENT CAPTURE AREAS RATHER THAN DISCHARGING TO FISH-BEARING STREAMS.
3. INSTALL SEINE NETS AT UPSTREAM END TO PREVENT FISH FROM MOVING DOWNSTREAM UNTIL 2/3 OF TOTAL FLOW IS RESTORED TO THE CHANNEL.
4. STARTING IN EARLY MORNING INTRODUCE 1/3 OF NEW CHANNEL FLOW OVER PERIOD OF 1-2 HOURS.
5. INTRODUCE SECOND THIRD OF FLOW OVER NEXT 1 TO 2 HOURS AND BEGIN FISH SALVAGE OF BYPASS CHANNEL IF FISH ARE PRESENT.
6. REMOVE UPSTREAM SEINE NETS ONCE 2/3 FLOW IN REWATERED CHANNEL AND DOWNSTREAM TURBIDITY IS WITHIN ACCEPTABLE RANGE (LESS THAN 40 NTU OR LESS THAN 10% BACKGROUND).
7. INTRODUCE FINAL THIRD OF FLOW ONCE FISH SALVAGE EFFORTS ARE COMPLETE AND DOWNSTREAM TURBIDITY VERIFIED TO BE WITHIN ACCEPTABLE RANGE.
8. INSTALL PLUG TO BLOCK FLOW INTO OLD CHANNEL OR BYPASS. REMOVE ANY REMAINING SEINE NETS.
9. IN LAMPREY SYSTEMS, LAMPREY SALVAGE AND DRY SHOCKING MAY BE NECESSARY.

TURBIDITY MONITORING.

- A. RECORD THE READING, LOCATION, AND TIME FOR THE BACKGROUND READING APPROXIMATELY 100 FEET UPSTREAM OF THE PROJECT AREA USING A RECENTLY CALIBRATED TURBIDIMETER OR VIA VISUAL OBSERVATION (SEE THE HIP HANDBOOK TURBIDITY MONITORING SECTION FOR A VISUAL OBSERVATION KEY).
- B. RECORD THE TURBIDITY READING, LOCATION, AND TIME AT THE MEASUREMENT COMPLIANCE LOCATION POINT.
 1. 50 FEET DOWNSTREAM FOR STREAMS LESS THAN 30 FEET WIDE.
 2. 100 FEET DOWNSTREAM FOR STREAMS BETWEEN 30 AND 100 FEET WIDE.
 3. 200 FEET DOWNSTREAM FOR STREAMS GREATER THAN 100 FEET WIDE.
 4. 300 FEET FROM THE DISCHARGE POINT OR NONPOINT SOURCE FOR LOCATIONS SUBJECT TO TIDAL OR COASTAL SCOUR.
- C. TURBIDITY SHALL BE MEASURED (BACKGROUND LOCATION AND COMPLIANCE POINTS) EVERY 4 HOURS WHILE WORK IS BEING IMPLEMENTED.
- D. IF THERE IS A VISIBLE DIFFERENCE BETWEEN A COMPLIANCE POINT AND THE BACKGROUND, THE EXCEEDANCE WILL BE NOTED IN THE PROJECT COMPLETION FORM (PCF). ADJUSTMENTS OR CORRECTIVE MEASURES WILL BE TAKEN IN ORDER TO REDUCE TURBIDITY.
- E. IF EXCEEDANCES OCCUR FOR MORE THAN TWO CONSECUTIVE MONITORING INTERVALS (AFTER 8 HOURS), THE ACTIVITY WILL STOP UNTIL THE TURBIDITY LEVEL RETURNS TO BACKGROUND. THE BPA EC LEAD WILL BE NOTIFIED OF ALL EXCEEDANCES AND CORRECTIVE ACTIONS AT PROJECT COMPLETION.
- F. IF TURBIDITY CONTROLS (COFFER DAMS, WADDLES, FENCING, ETC.) ARE DETERMINED INEFFECTIVE, CREWS WILL BE MOBILIZED TO MODIFY AS NECESSARY. OCCURRENCES WILL BE DOCUMENTED IN THE PROJECT COMPLETION FORM (PCF).
- G. FINAL TURBIDITY READINGS, EXCEEDANCES, AND CONTROL FAILURES WILL BE SUBMITTED TO THE BPA EC LEAD USING THE PROJECT COMPLETION FORM (PCF).

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RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**



**HIP GENERAL CONSERVATION
MEASURES (3 OF 3)**

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SURVEY CONTROL

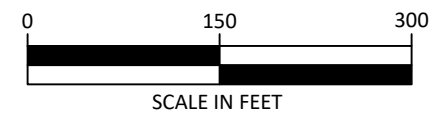
POINT #	NORTHING	EASTING	ELEVATION	DESCRIPTION
101	933852.32	7415483.64	12.52	NAIL
100	933693.34	7415178.59	12.24	NAIL
999	933568.67	7414938.24	12.45	NAIL

LEGEND

- EXISTING CONTOURS (1FT)
- TAXLOTS (FROM CLATSOP COUNTY GIS)
- EXISTING RAILROAD TRACKS
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)
- NWI WETLAND
- CONTROL POINT

NOTE:

INUNDATION EXTENTS ARE BASED ON EXISTING TOPOGRAPHIC CONTOURS. ACTUAL SITE INUNDATION MAY VARY DUE TO IMPAIRED CONNECTIVITY.



NO.	BY	DATE	REVISION DESCRIPTION

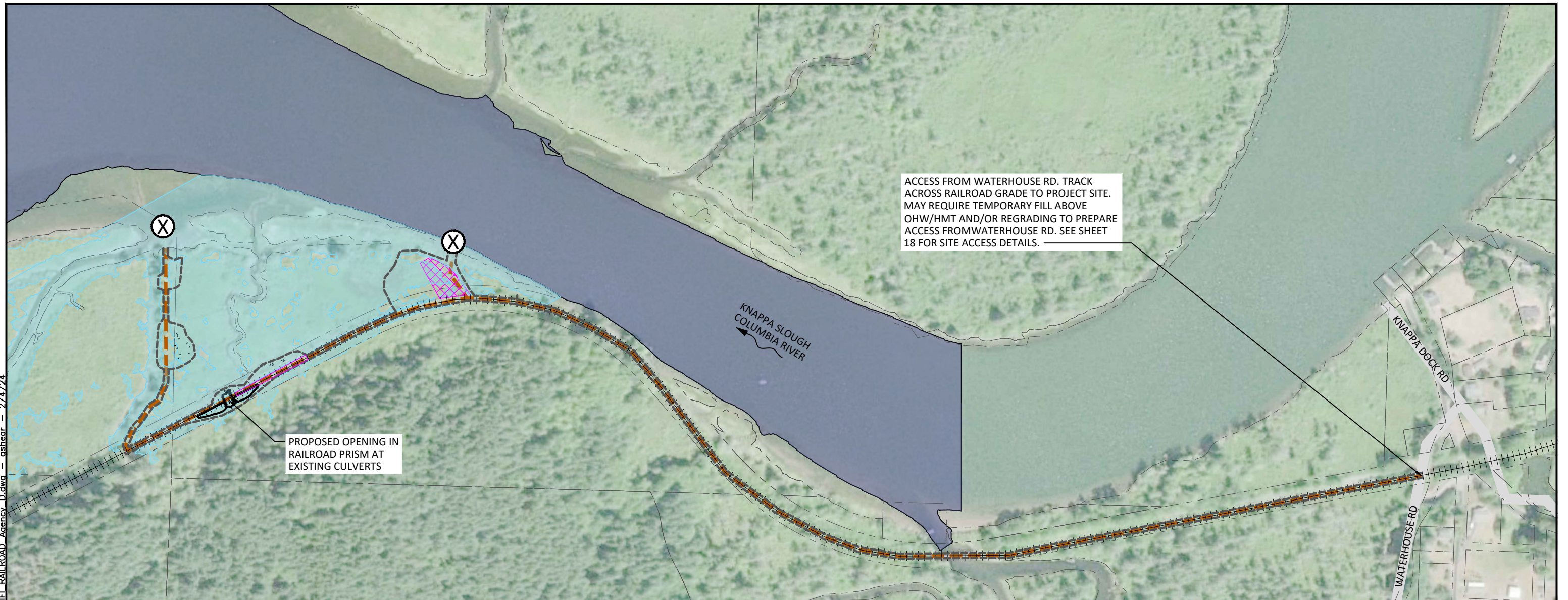
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**



**EXISTING CONDITIONS
& SURVEY CONTROL**

G:\0-T-Railroad_AgencyCreekWarrenSlough_CREST_190220\Drawings\11_AGENCY_CREEK_FINAL_RAILROAD_Agency D.dwg - 2/4/24



ACCESS FROM WATERHOUSE RD. TRACK ACROSS RAILROAD GRADE TO PROJECT SITE. MAY REQUIRE TEMPORARY FILL ABOVE OHW/HMT AND/OR REGRADING TO PREPARE ACCESS FROM WATERHOUSE RD. SEE SHEET 18 FOR SITE ACCESS DETAILS.

PROPOSED OPENING IN RAILROAD PRISM AT EXISTING CULVERTS

LEGEND

- ===== EXISTING RAILROAD TRACKS
- LIMIT OF DISTURBANCE
- - - - - TAXLOTS (FROM CLATSOP COUNTY GIS)
- TEMPORARY ACCESS ROUTE
- XXXXXX TEMPORARY STAGING AREA / NATURAL MATERIAL STOCKPILE AREA
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)
- (X) APPROXIMATE BARGE ACCESS LOCATION

NOTES:

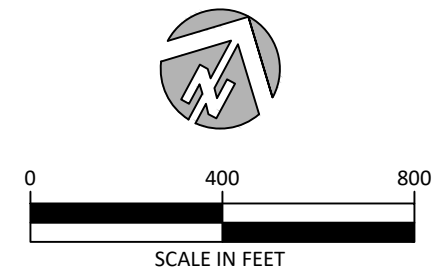
ACCESS MAY REQUIRE MINOR EARTHWORK TO FACILITATE MOVEMENT OF EQUIPMENT FROM BARGE TO LEVEE.

VARIATION IN ACCESS LOCATION(S) MAY BE REQUIRED BASED ON LOCAL BED TOPOGRAPHY AT TIME OF CONSTRUCTION.

SEE DETAIL 1, SHEET 18 FOR RAILROAD OVERLAND ACCESS ALTERNATIVES.

LIMITS OF DISTURBANCE WILL BE MARKED IN THE FIELD APPROXIMATELY EVERY 20 FEET. FLAGGING WILL BE ATTACHED TO THE HIGHEST POINT POSSIBLE IN EXISTING VEGETATION TO FACILITATE VISIBILITY.

TIDAL DATUMS ARE SHOWN FOR THE PROJECT AREA ONLY.



NO.	BY	DATE	REVISION DESCRIPTION

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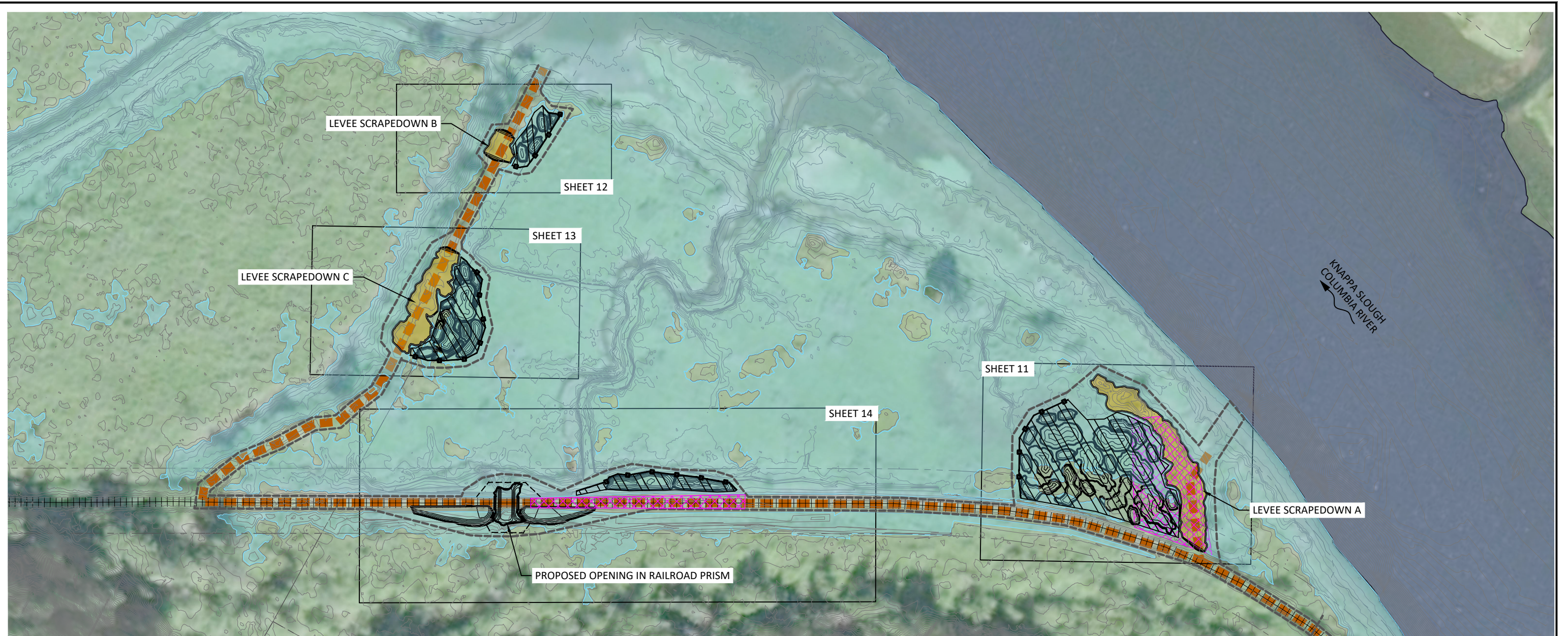
COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN




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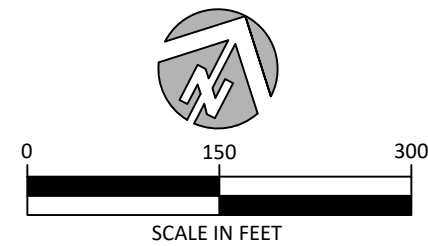
TEMPORARY ACCESS

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LEGEND

- | | | | |
|--|---|--|--|
| | EXISTING CONTOURS (1FT) | | LIMIT OF DISTURBANCE |
| | PROPOSED CONTOURS (1FT) | | TURBIDITY CURTAIN (SEE DETAIL 1, SHEET 7) |
| | GRADING BOUNDARY | | STRAW WATTLES (SEE DETAIL 2, SHEET 7) |
| | TAXLOTS (FROM CLATSOP COUNTY GIS) | | MEAN HIGHER HIGH WATER INUNDATION (8.88FT) |
| | EXISTING RAILROAD TRACKS | | MEAN LOWER LOW WATER INUNDATION (0.73FT) |
| | TEMPORARY ACCESS ROUTE | | LEVEE SCRAPEDOWN AREA |
| | TOPOGRAPHIC COMPLEXITY ZONE (SEE SHEET 21) | | |
| | PROPOSED LEVEE SHOULDER VEGETATION COMPLEXITY ZONE (SEE DETAIL 1, SHEET 22) | | |
| | NATURAL MATERIAL STOCKPILE AREA | | |



NOTES:
 PLACE NATURAL MATERIALS TO MINIMIZE DISTURBANCE TO NATIVE VEGETATION.
 SEE SHEET 15 FOR PROPOSED OPENING PROFILE
 SEE SHEET 18 FOR SITE ACCESS DETAILS
 INUNDATION EXTENTS ARE BASED ON EXISTING TOPOGRAPHIC CONTOURS. ACTUAL SITE INUNDATION MAY VARY DUE TO IMPAIRED CONNECTIVITY.

GS	MC, CA, MB	MB
DRAWN	DESIGNED	CHECKED
MC	02/2024	190220
APPROVED	DATE	PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
 RAILROAD RESTORATION
 AGENCY CREEK - FINAL DESIGN**

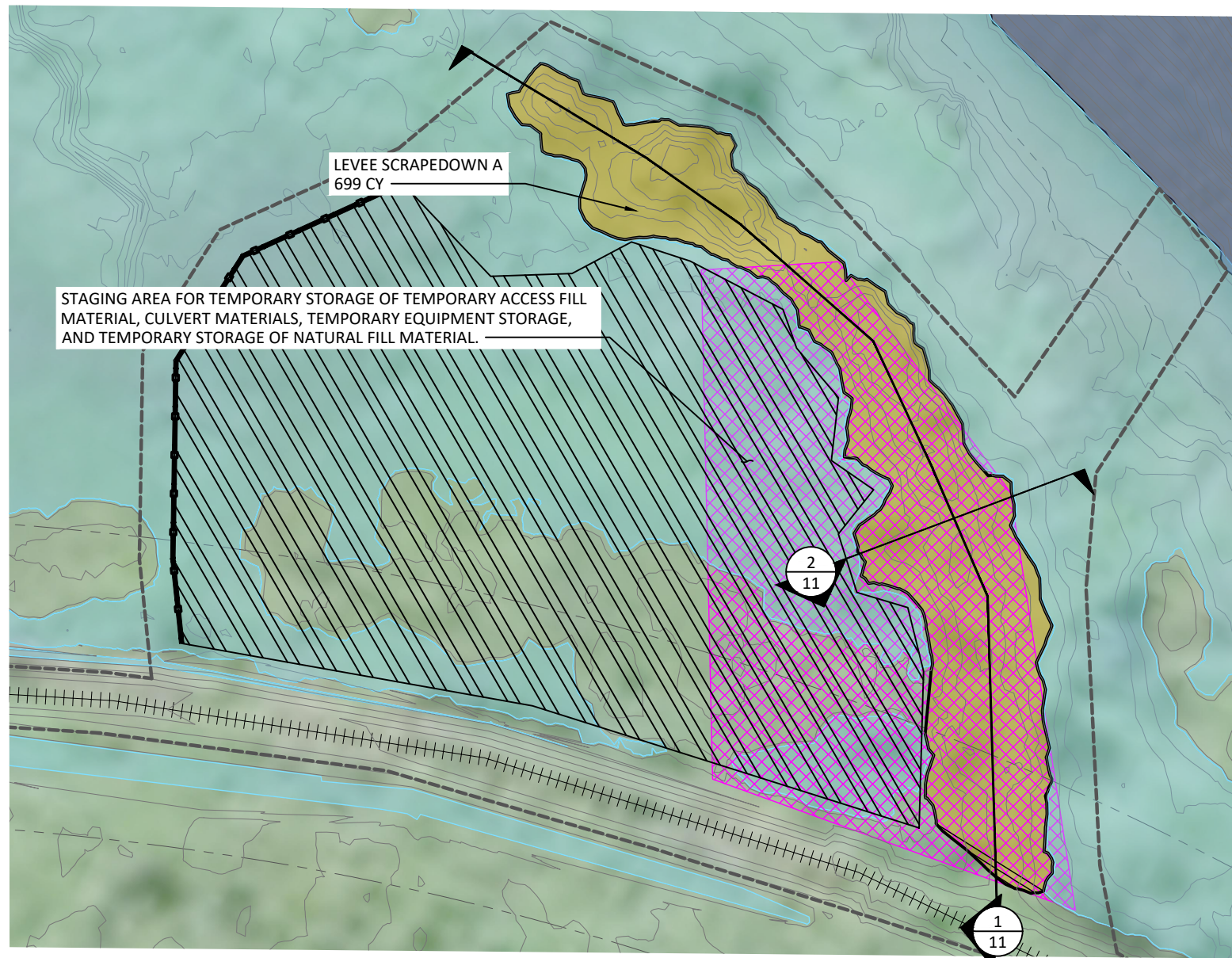



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**PROPOSED CONDITIONS
 OVERVIEW & INDEX**

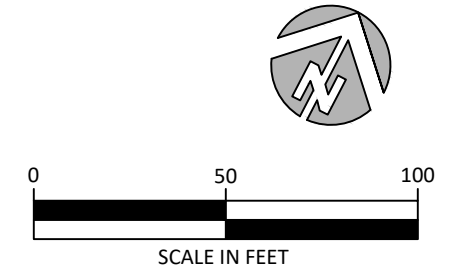
NO.	BY	DATE	REVISION DESCRIPTION

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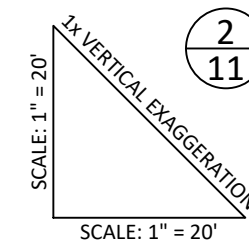
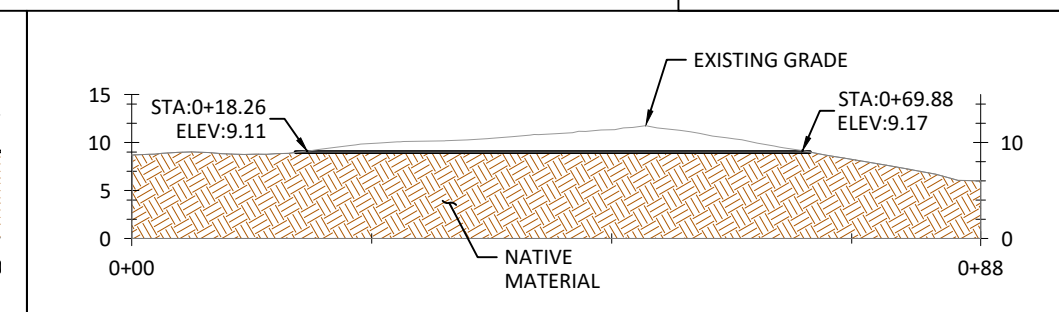
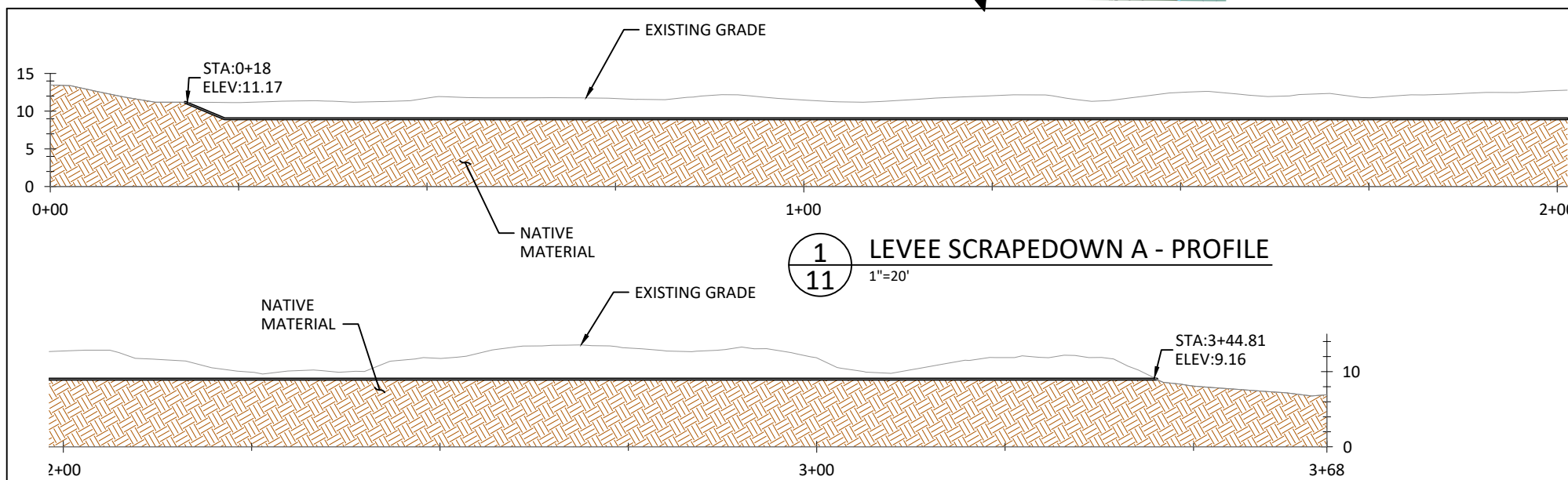
LEGEND

- EXISTING CONTOURS (1FT)
- PROPOSED CONTOURS (1FT)
- GRADING BOUNDARY
- TAXLOTS (FROM CLATSOP COUNTY GIS)
- EXISTING RAILROAD TRACKS
- TOPOGRAPHIC COMPLEXITY ZONE (SEE SHEET 21)
- LIMIT OF DISTURBANCE
- STRAW WATTLE (SEE DETAIL 2, SHEET 7)
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)
- NATURAL MATERIAL STOCKPILE AREA
- LEVEE SCRAPEDOWN AREA



DATUM LINE LEGEND

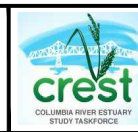
- EXISTING GROUND
- PROPOSED GRADING



NO.	BY	DATE	REVISION DESCRIPTION

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MC APPROVED	02/2024 DATE	190220 PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**

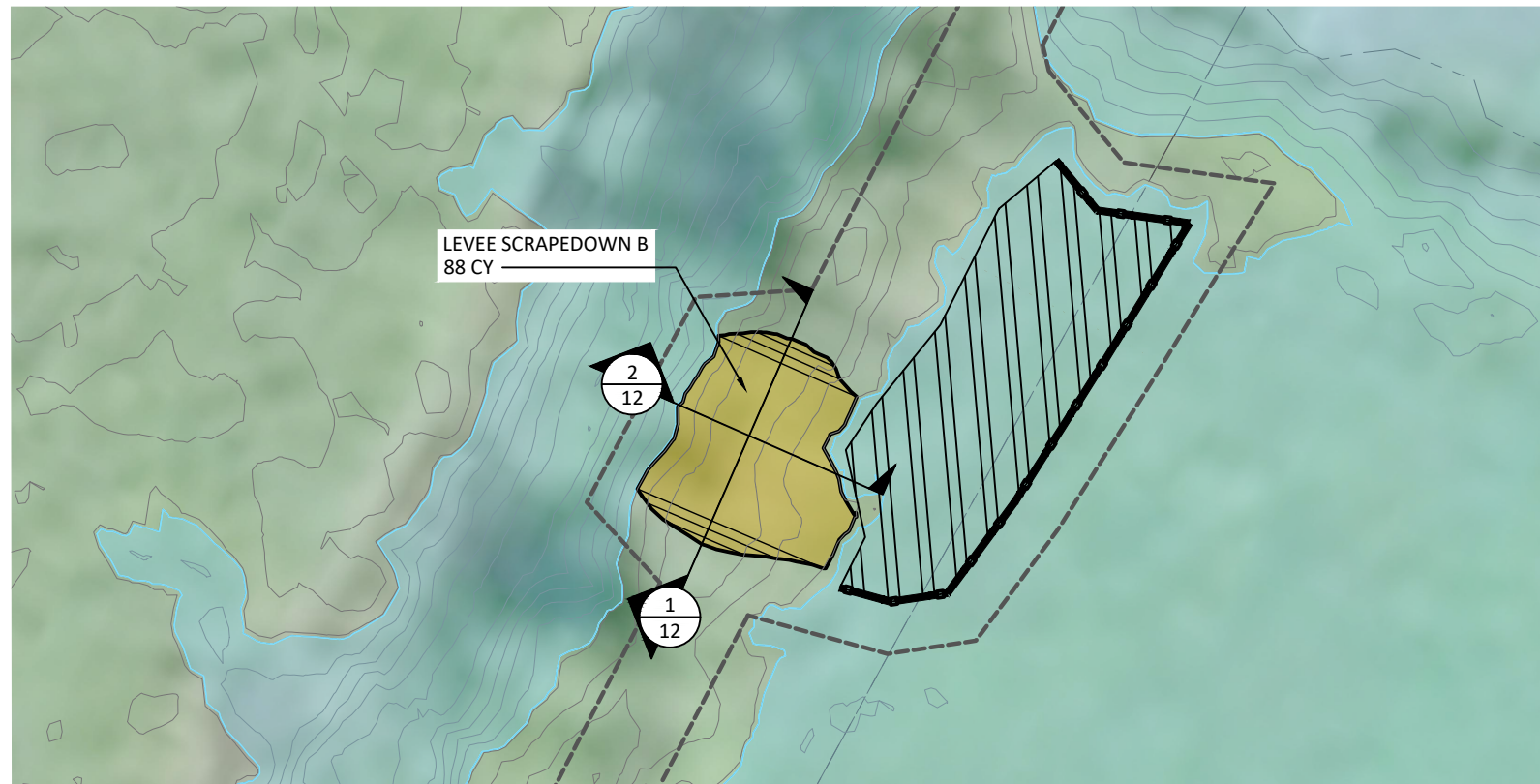


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**PROPOSED CONDITIONS
LEVEE A**

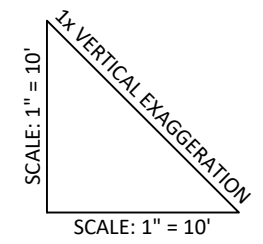
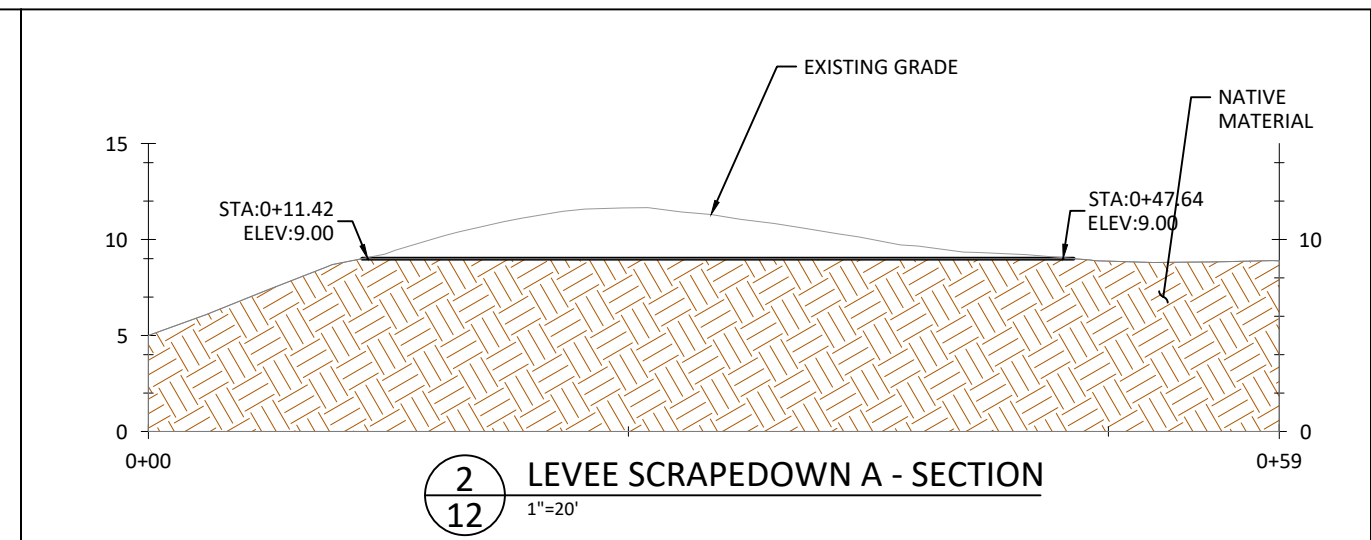
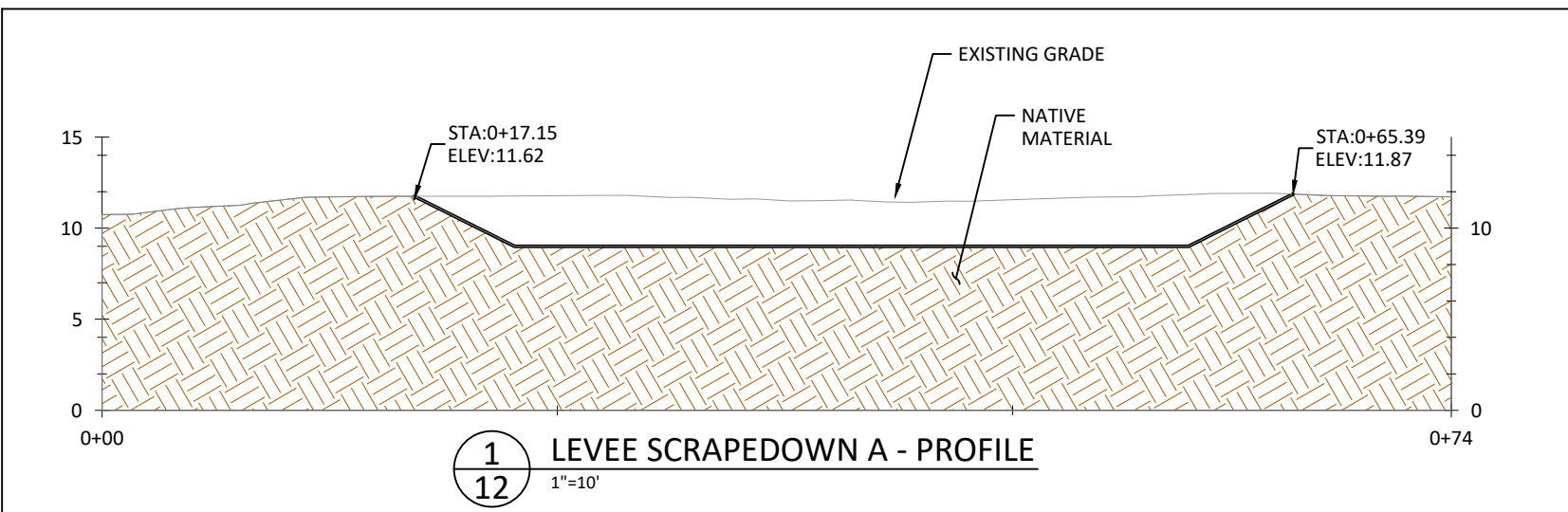
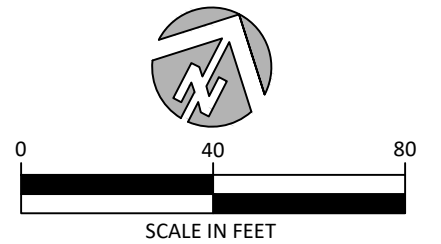
SHEET
11 OF 40

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LEGEND

- EXISTING CONTOURS (1FT)
- PROPOSED CONTOURS (1FT)
- GRADING LIMIT
- TAXLOTS (FROM CLATSOP COUNTY GIS)
- TOPOGRAPHIC COMPLEXITY ZONE (SEE SHEET 21)
- LIMIT OF DISTURBANCE
- STRAW WATTLE (SEE DETAIL 2, SHEET 7)
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)
- LEVEE SCRAPEDOWN AREA



DATUM LINE LEGEND

- EXISTING GROUND
- PROPOSED GRADING

NO.	BY	DATE	REVISION DESCRIPTION

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AGENCY CREEK - FINAL DESIGN**

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**PROPOSED CONDITIONS
LEVEE B**

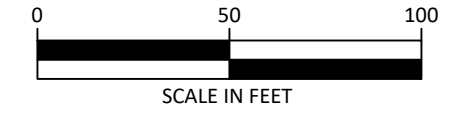
SHEET
12 OF 40

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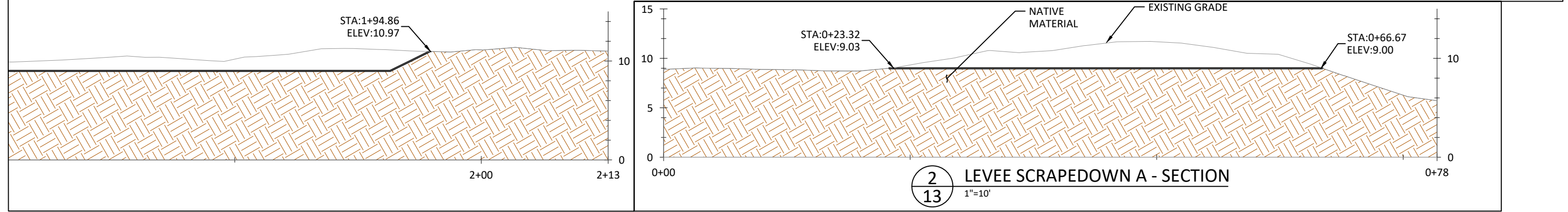
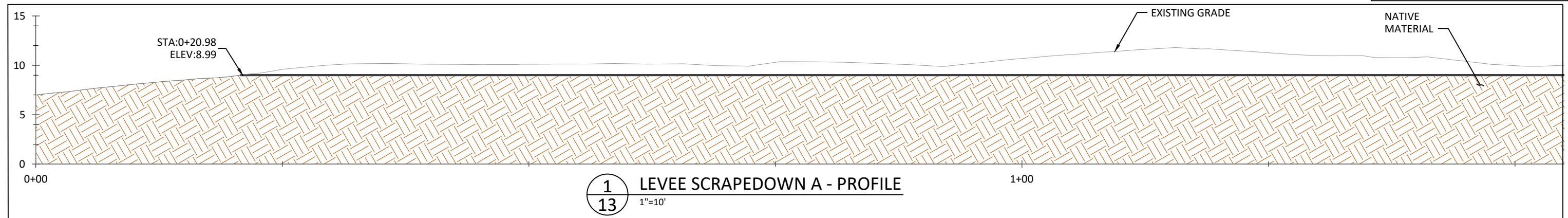
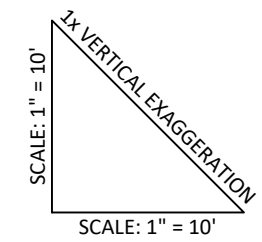
LEGEND

- EXISTING CONTOURS (1FT)
- PROPOSED CONTOURS (1FT)
- GRADING LIMIT
- TAXLOTS (FROM CLATSOP COUNTY GIS)
- TOPOGRAPHIC COMPLEXITY ZONE (SEE SHEET 21)
- LIMIT OF DISTURBANCE
- STRAW WATTLE (SEE DETAIL 2, SHEET 7)
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)
- LEVEE SCRAPEDOWN AREA



DATUM LINE LEGEND

- EXISTING GROUND
- PROPOSED GRADING



NO.	BY	DATE	REVISION DESCRIPTION

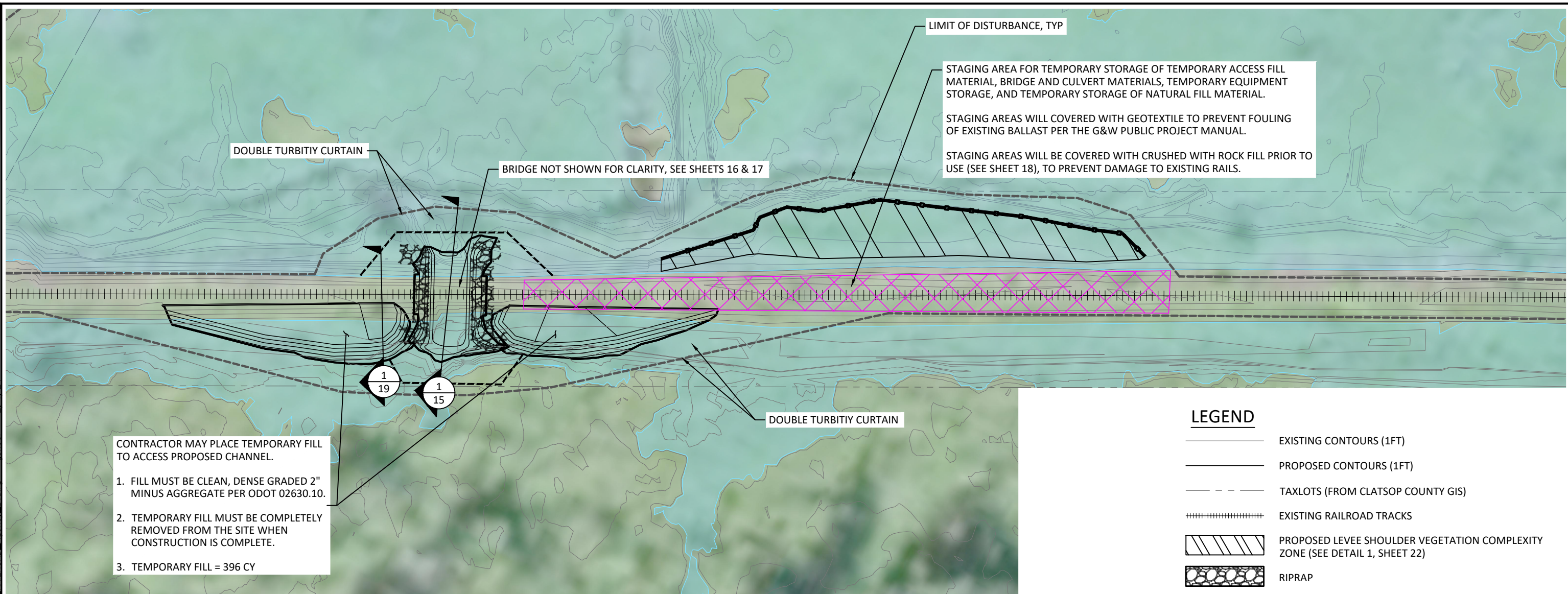
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

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RAILROAD RESTORATION
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**PROPOSED CONDITIONS
LEVEE C**

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CONTRACTOR MAY PLACE TEMPORARY FILL TO ACCESS PROPOSED CHANNEL.

1. FILL MUST BE CLEAN, DENSE GRADED 2" MINUS AGGREGATE PER ODOT 02630.10.
2. TEMPORARY FILL MUST BE COMPLETELY REMOVED FROM THE SITE WHEN CONSTRUCTION IS COMPLETE.
3. TEMPORARY FILL = 396 CY

LIMIT OF DISTURBANCE, TYP

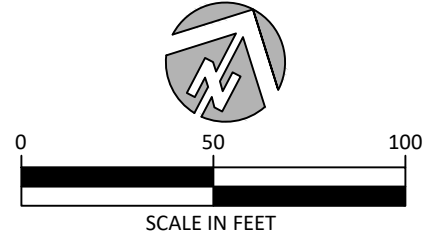
STAGING AREA FOR TEMPORARY STORAGE OF TEMPORARY ACCESS FILL MATERIAL, BRIDGE AND CULVERT MATERIALS, TEMPORARY EQUIPMENT STORAGE, AND TEMPORARY STORAGE OF NATURAL FILL MATERIAL.

STAGING AREAS WILL COVERED WITH GEOTEXTILE TO PREVENT FOULING OF EXISTING BALLAST PER THE G&W PUBLIC PROJECT MANUAL.

STAGING AREAS WILL BE COVERED WITH CRUSHED WITH ROCK FILL PRIOR TO USE (SEE SHEET 18), TO PREVENT DAMAGE TO EXISTING RAILS.

LEGEND

- EXISTING CONTOURS (1FT)
- PROPOSED CONTOURS (1FT)
- TAXLOTS (FROM CLATSOP COUNTY GIS)
- EXISTING RAILROAD TRACKS
- PROPOSED LEVEE SHOULDER VEGETATION COMPLEXITY ZONE (SEE DETAIL 1, SHEET 22)
- RIPRAP
- LIMIT OF DISTURBANCE
- TURBIDITY CURTAIN (SEE DETAIL 1, SHEET 7)
- STRAW WATTLE. (SEE DETAIL 2, SHEET 7)
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)



PROPOSED SEQUENCING PLAN

1. ACCESS SITE ALONG RAILROAD, STAGE MATERIAL IN APPROVED SITE.
2. REMOVE RAILS IN ZONE OF EXCAVATION.
3. INSTALL TURBIDITY CURTAINS ON INTERIOR AND EXTERIOR OF THE BREACH LOCATION.
4. PREPARE GROUND AS NECESSARY TO ALLOW FOR INSTALLATION OF H-PILES AND PILEUAPS
5. INSTALL H-PILES.
6. INSTALL PILE CAPS.
7. EXCAVATE REMAINDER OF CHANNEL AND APPLY BANK ARMOR.
8. INSTALL BRIDGE DECK (MAY BE INSTALLED CONCURRENTLY IN STEP 7, DEPENDING ON PREFERENCE OF CONTRACTOR.
9. REMOVE TEMPORARY ACCESS FILL.
10. REPLACE RAILS.
11. DEMOBILIZE.

NOTES:
 CONTRACTOR SHALL HAVE STRAW WATTLES AVAILABLE ON SITE FOR PLACEMENT IN ADDITIONAL LOCATIONS WHERE RUNOFF FROM DISTURBED GROUND MAY CONTACT SURFACE WATERS, IN COORDINATION WITH PROJECT OWNER.

SEE SHEET 18 FOR SITE ACCESS DETAILS.

MATERIAL EXCAVATED FROM WITHIN THE G AND W ROW SHALL BE PLACED WITHIN THE ROW. NO EXPORT OF MATERIAL FROM THE ROW TO ADJACENT PROPERTY SHALL BE ALLOWED.

NO.	BY	DATE	REVISION DESCRIPTION

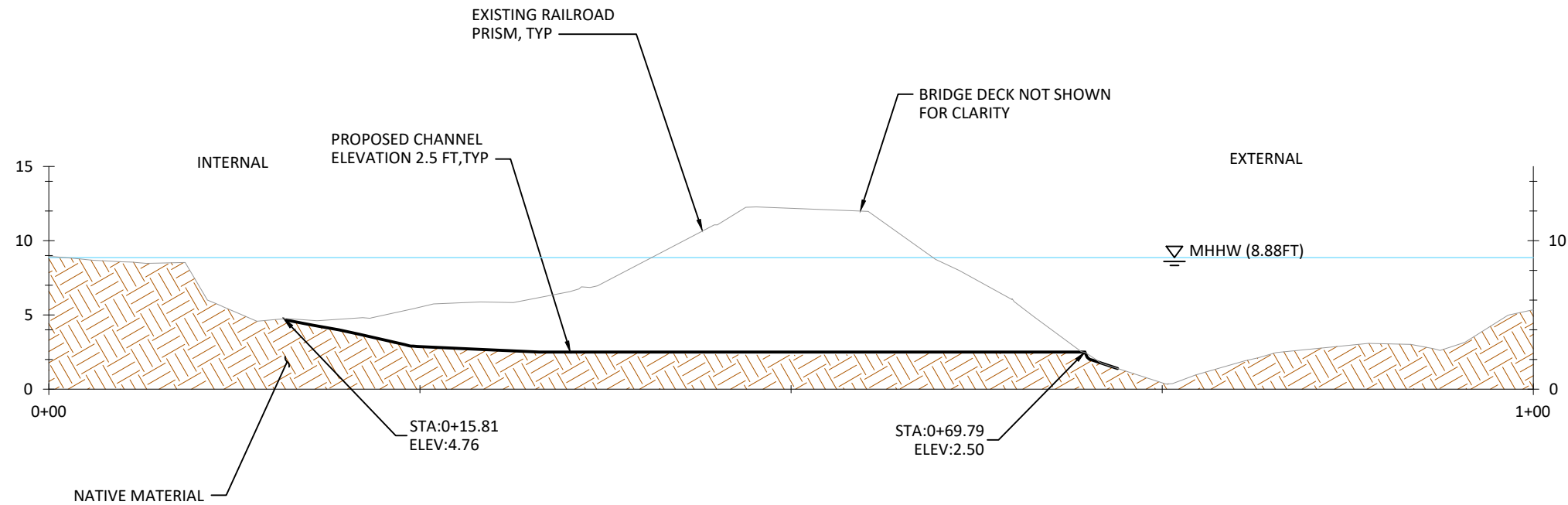
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
 RAILROAD RESTORATION
 AGENCY CREEK - FINAL DESIGN**

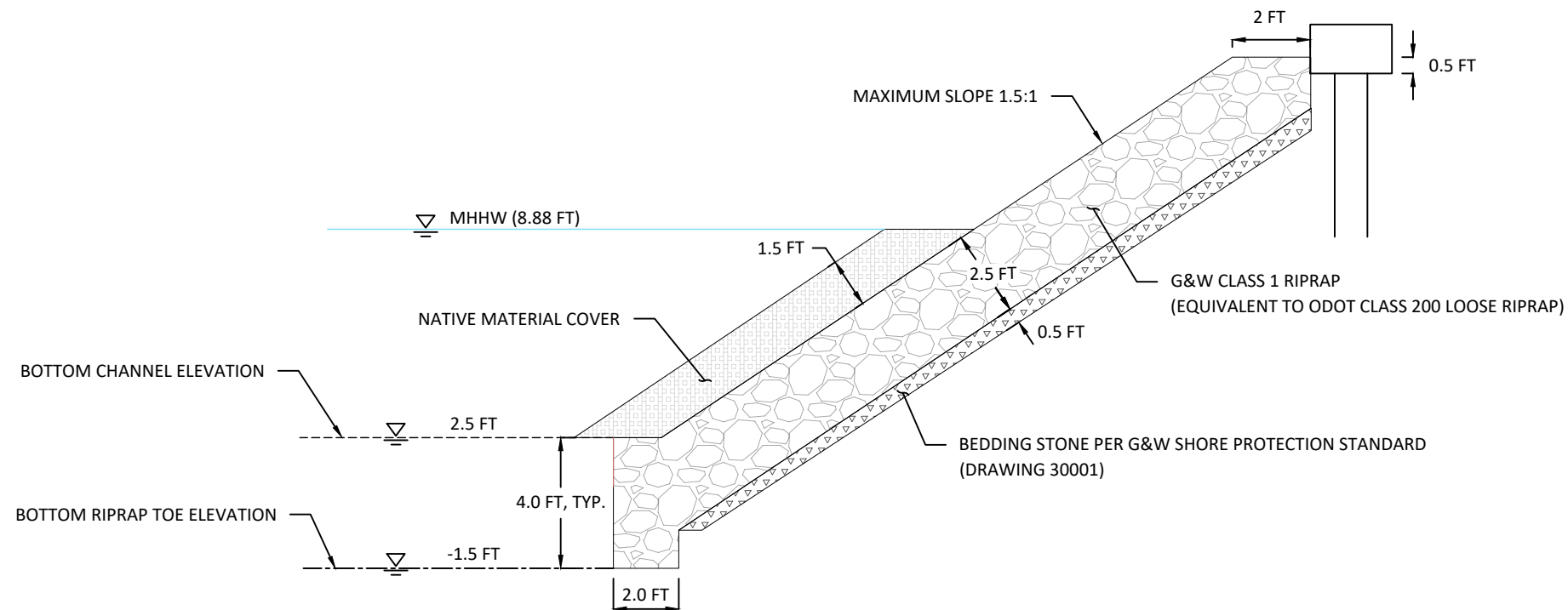
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**PROPOSED CONDITIONS
 OPENING**

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1
15 BRIDGE SPAN PROFILE
1" = 10'



2
15 BANK PROTECTION CROSS SECTION - OPENING
NOT TO SCALE

DATUM LINE LEGEND

- EXISTING GROUND
- PROPOSED GRADING

1x VERTICAL EXAGGERATION
SCALE: 1" = 10'
SCALE: 1" = 10'

NO.	BY	DATE	REVISION DESCRIPTION

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MC APPROVED	02/2024 DATE	190220 PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



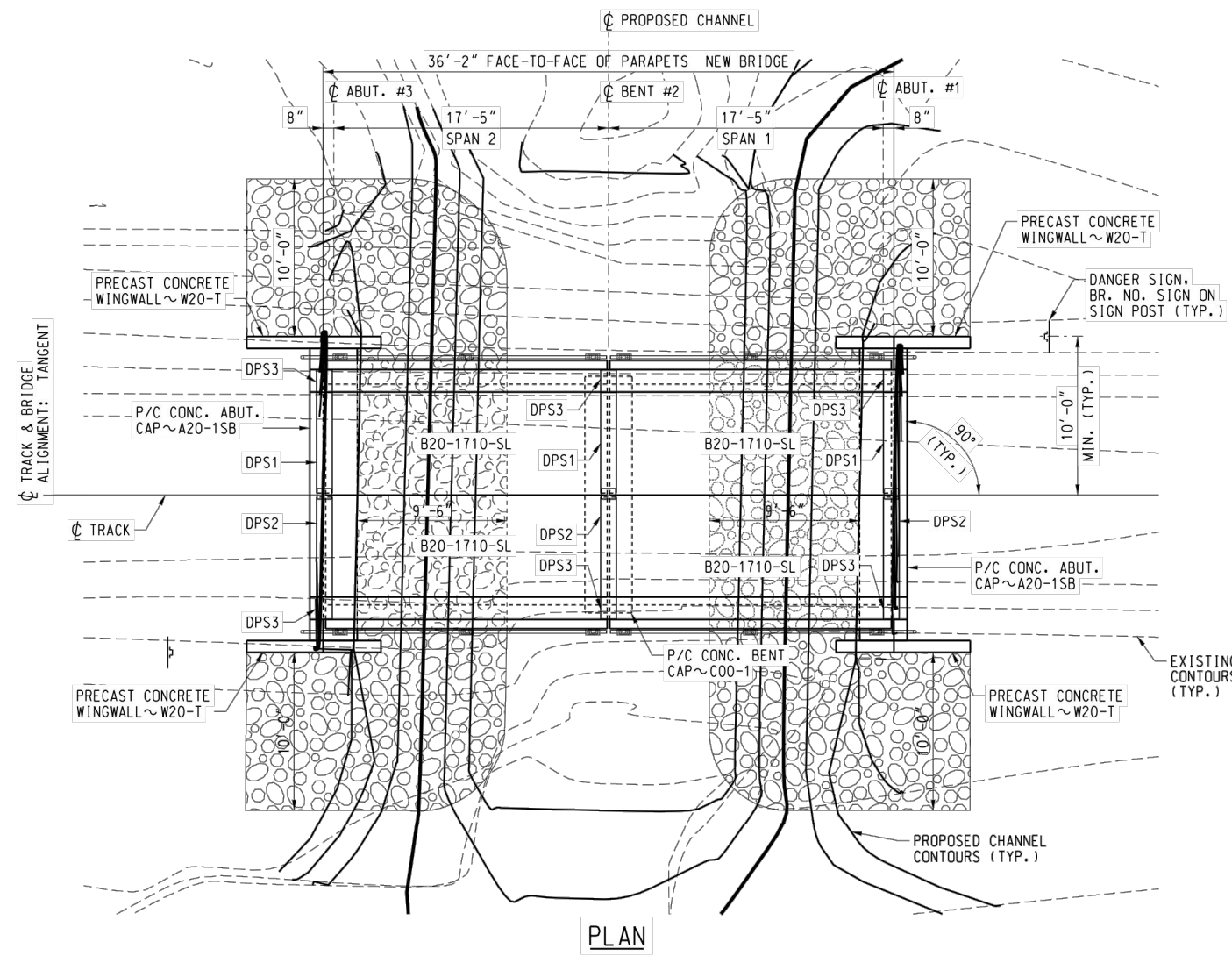
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BRIDGE SPAN PROFILE AND
BANK PROTECTION SECTION

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WEST
TO ASTORIA

EAST
TO ALDRICH



LIST OF DRAWINGS - AGENCY CREEK BRIDGE	
PLAN NO.	TITLE
001	GENERAL PLAN & ELEVATION
002	PILE LAYOUT PLAN & TYPICAL SECTIONS

TABLE OF EST. LIFTING WEIGHTS		
ITEM	MARK NO.	ESTIMATED WEIGHT (LBS)
PRECAST P/S CONC. SLAB BEAM	B20-1710-SL	36,915
PRECAST CONC. ABUTMENT CAP	A20-1SB	23,910
PRECAST CONC. WINGWALL	W20-T	4,470
PRECAST CONC. PIER CAP	C00-1	20,060

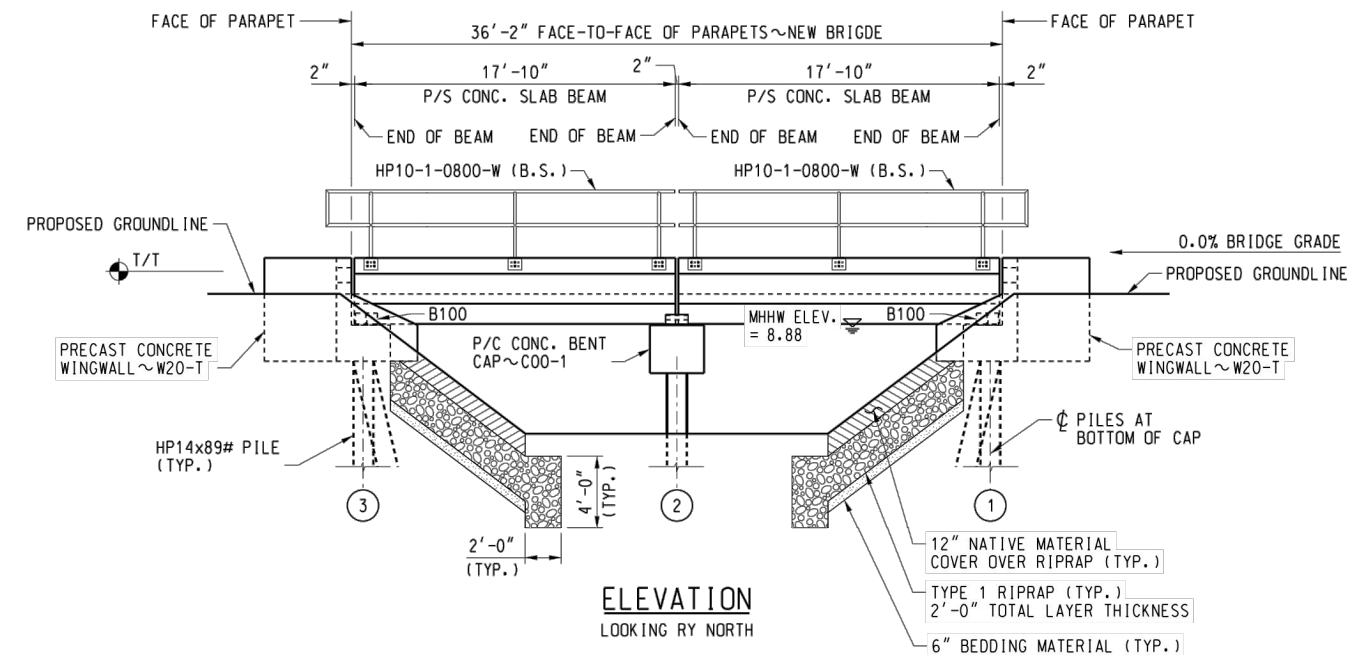
TABLE OF ELEVATION				
LOCATION	TOP/TIE	TOP/CAP	PILE CUTOFF	T/T TO PILE CUTOFF
ABUT. 1	12.00	9.02	7.02	4'-11 3/4"
BENT 2	12.00	9.02	6.35	5'-7 3/4"
ABUT. 3	12.00	9.02	7.02	4'-11 3/4"

BENCH MARK:
SEE CIVIL PLANS

LEGEND:
T/T = TOP OF TIE
RY = RAILWAY
B.S. = BOTH SIDES
W = WITH WALK

GENERAL NOTES:

RIPRAP SHALL BE PLACED IN SUCH A MANNER AS TO AVOID SEGREGATION OF VARIOUS SIZES OF ROCK AND DISTRIBUTED SO THAT THERE WILL BE NO LARGE ACCUMULATION OF EITHER THE LARGER OR SMALLER SIZES OF STONE. INDIVIDUAL ROCKS SHALL BE PLACED IN TIGHT CONTACT WITH ONE ANOTHER IN SUCH A WAY TO PRODUCE THE LEAST AMOUNT OF VOID SPACES. RIPRAP SHALL BE SOLID, UNFRACTURED ROCK, BULKY IN SHAPE WITH SHARP ANGULAR EDGES. THE ENTIRE MASS OF RIPRAP SHALL BE WELL DISTRIBUTED WITHIN THE LIMITS SPECIFIED.



DESIGN DATA:

2020 A.R.E.M.A. DESIGN SPECIFICATIONS
LOADING: COOPER E 80 W/ DIESEL IMPACT

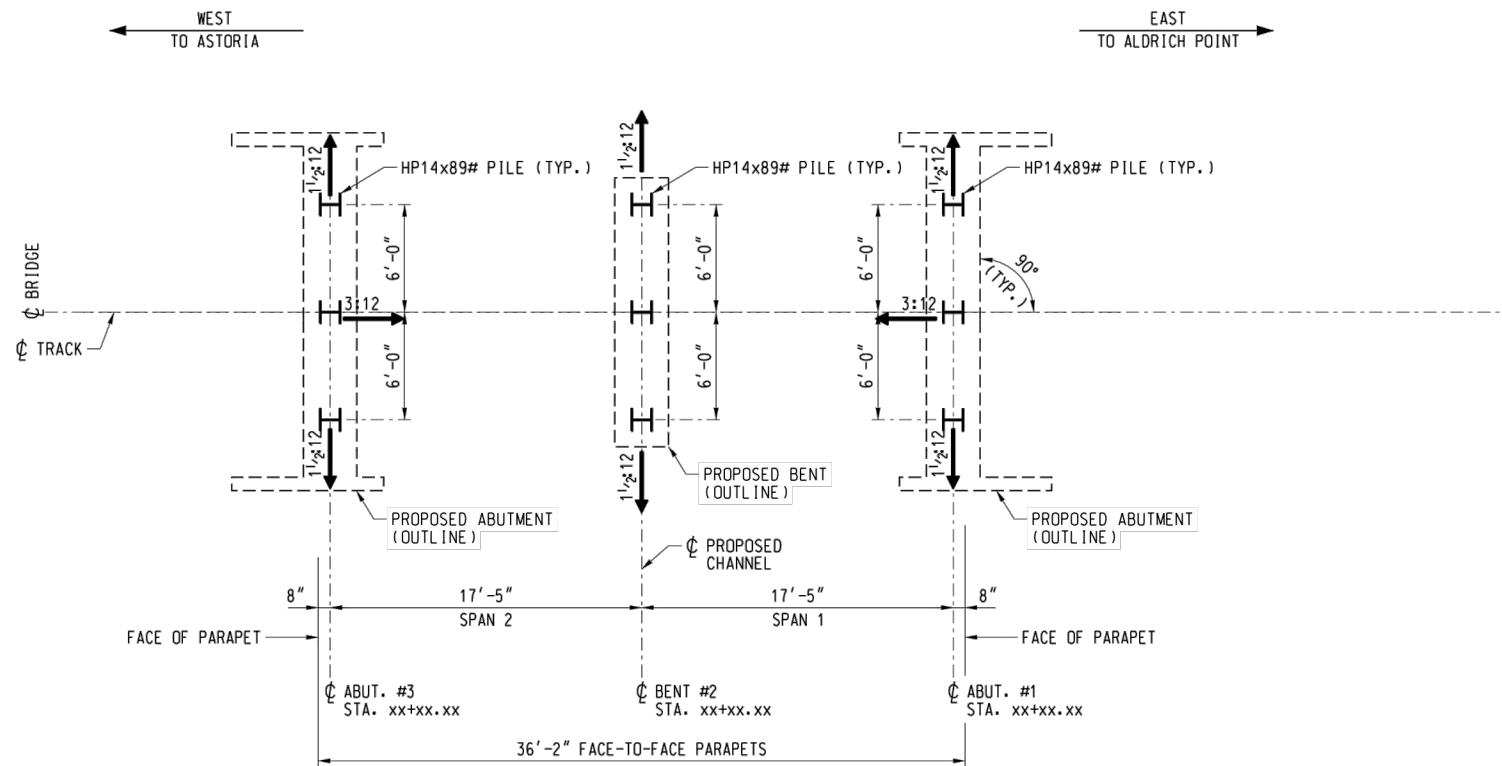
Preliminary
Not for Construction

GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
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COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



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PILE LAYOUT PLAN



Know what's below.
Call before you dig.

ATTENTION !

INFORMATION SHOWN ON THESE PLANS CONCERNING TYPE AND LOCATION OF UNDERGROUND OR ABOVE GROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL INCLUSIVE.

THE SUPERVISOR OF STRUCTURES OR THE FOREMAN IN CHARGE WILL VERIFY THE LOCATION OF UNDERGROUND AND OVERHEAD UTILITIES BEFORE BEGINNING CONSTRUCTION.

PILE NOTES:

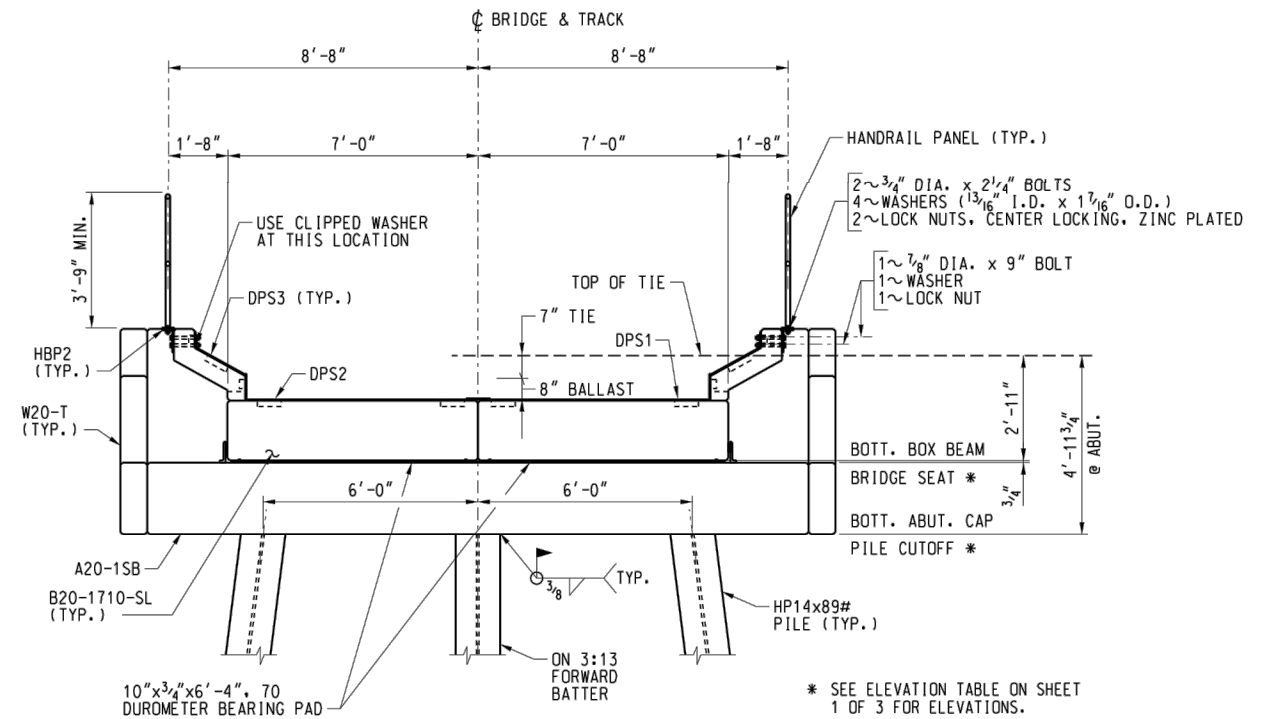
PILES SHALL BE DRIVEN TO REFUSAL, IF POSSIBLE OR TO A MINIMUM ULTIMATE RESISTANCE OF 250 TONS.

ALL PILES ARE TO BE DRIVEN WITH REINFORCED TIPS (PILE POINTS).

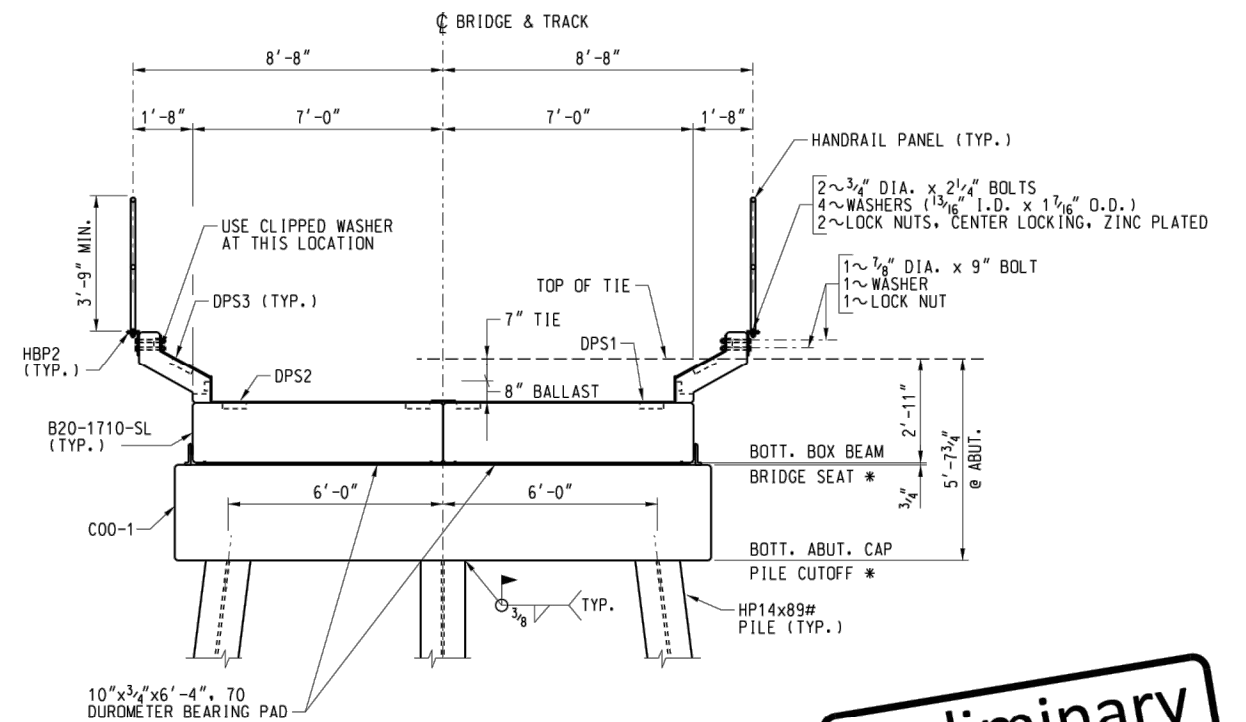
ESTIMATED PILE LENGTH BELOW CUTOFF = xx'.

PILE SPACING SHOWN ARE AT PILE CUTOFF ELEVATIONS.

SYMBOL X:12 DENOTES DIRECTION AND AMOUNT OF PILE BATTER.



TYPICAL CROSS SECTION (ABUTMENT)



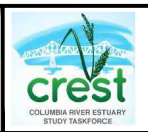
TYPICAL CROSS SECTION (BENT)

Preliminary
Not for Construction

NO.	BY	DATE	REVISION DESCRIPTION

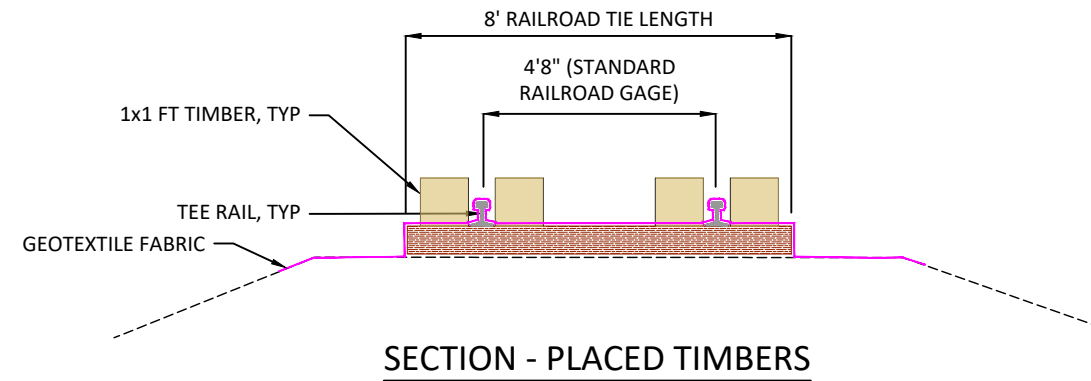
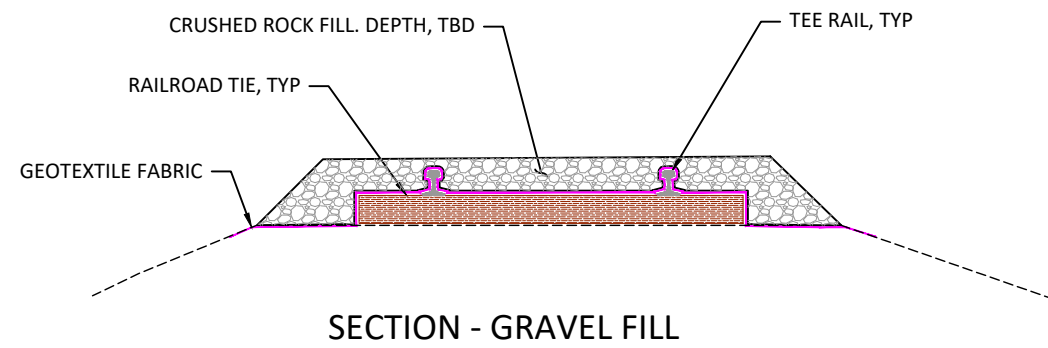
GS	MC, CA, MB	MB
DRAWN	DESIGNED	CHECKED
MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



BRIDGE TYPICAL DETAILS

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NOTE:

1. GEOTEXTILE TO PROTECT BALLAST FROM FOULING IN ACCESS AREAS IS REQUIRED PER G&W PUBLIC PROJECT MANUAL
2. ACCESS ACROSS RAIL MAY BE REQUIRED. RAILS MUST BE LEFT IN A SIMILAR OR BETTER CONDITION POST-CONSTRUCTION. PLACED TIMBERS OR GRAVEL FILL MAY BE USED TO PROTECT RAIL, AND ALLOW ACCESS BY EQUIPMENT. ACCESS METHODS TO BE APPROVED BY CREST AND GENESEE AND WYOMING PRIOR TO CONSTRUCTION.

1
18 TYPICAL DETAIL - RAILROAD OVERLAND ACCESS OPTIONS
1" = 5'

NO.	BY	DATE	REVISION DESCRIPTION

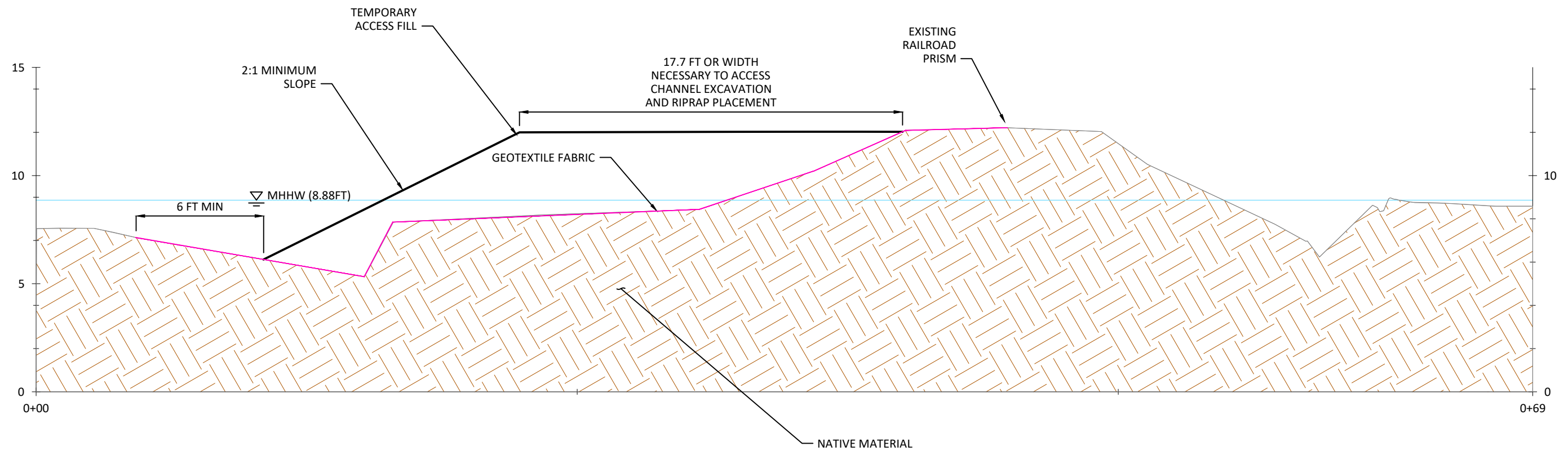
GS	MC, CA, MB	MB
DRAWN	DESIGNED	CHECKED
MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



SITE ACCESS & DETAILS

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1
19 TEMPORARY FILL PLACEMENT
NOT TO SCALE

NO.	BY	DATE	REVISION DESCRIPTION

GS	MC, CA, MB	MB
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MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



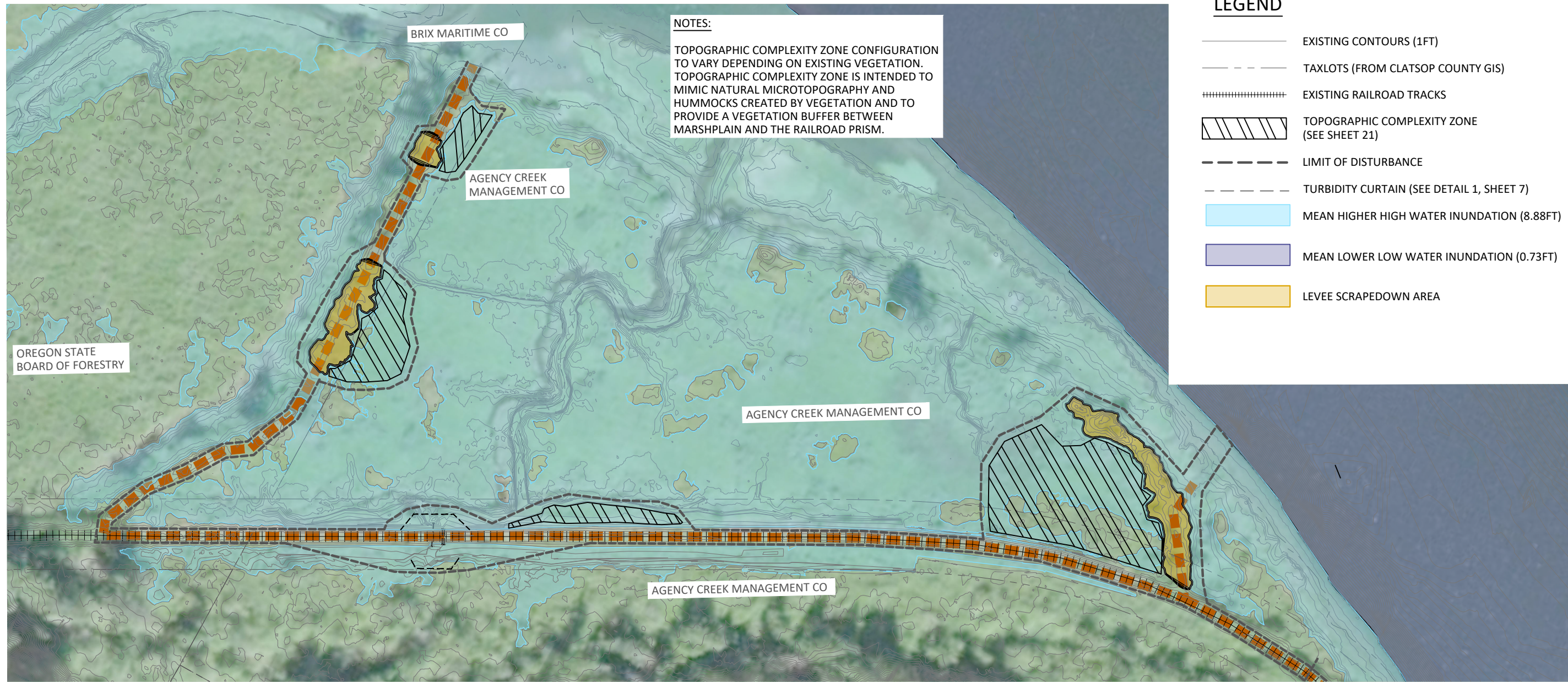
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Hood River, OR 97031
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TEMPORARY FILL ACCESS
DETAILS

SHEET

19 OF 40

G:\0 - Railroad_AgencyCreek\WarrenSlough_CREST_190220\Drawings\JEL_RAILROAD_Agency D.dwg - asheor - 2/4/24



NOTES:
 TOPOGRAPHIC COMPLEXITY ZONE CONFIGURATION TO VARY DEPENDING ON EXISTING VEGETATION. TOPOGRAPHIC COMPLEXITY ZONE IS INTENDED TO MIMIC NATURAL MICROTOPOGRAPHY AND HUMMOCKS CREATED BY VEGETATION AND TO PROVIDE A VEGETATION BUFFER BETWEEN MARSHPLAIN AND THE RAILROAD PRISM.

LEGEND

- EXISTING CONTOURS (1FT)
- - - TAXLOTS (FROM CLATSOP COUNTY GIS)
- ===== EXISTING RAILROAD TRACKS
- ▨ TOPOGRAPHIC COMPLEXITY ZONE (SEE SHEET 21)
- - - - - LIMIT OF DISTURBANCE
- - - - - TURBIDITY CURTAIN (SEE DETAIL 1, SHEET 7)
- MEAN HIGHER HIGH WATER INUNDATION (8.88FT)
- MEAN LOWER LOW WATER INUNDATION (0.73FT)
- LEVEE SCRAPEDOWN AREA

TOPOGRAPHIC COMPLEXITY PLANTING (above 8 feet elevation, NAVD88)

Botanical Name	Common Name	Stock Type	Stock Size	Average Spacing (on center)	Total Plants
<i>Cornus sericea</i>	Red osier dogwood	plug	16 cu. in.	4'	800
<i>Spiraea douglasii</i>	Douglas spirea	plug	16 cu. in.	4'	800
<i>Physocarpus capitatus</i>	Pacific ninebark	plug	16 cu. in.	4'	800
<i>Lonicera involucrata</i>	Twinberry	plug	16 cu. in.	4'	800
<i>Salix lasiandra</i>	Pacific willow	live cutting	5', 1-1.5" dia	4'	1600
<i>Salix sitchensis</i>	Sitka willow	live cutting	5', 1-1.5" dia	4'	1600
<i>Picea sitchensis</i>	Sitka spruce	potted plant	24"	See Note	200

SEED MIX APPLY AT A RATE OF 20 LBS/ACRE

Botanical Name	Common Name	Percent Composition
<i>Sercale cereale</i>	Cereal Rye (Cover Crop)	30%
<i>Glyceria occidentalis</i>	Western Mannagrass	25%
<i>Hordeum brachyantherum</i>	Meadow Barley	20%
<i>Beckmannia syzigachne</i>	American Sloughgrass	15%
<i>Deschampsia caespitosa</i>	Tufted Hairgrass	10%

NOTES:

1. ALL DISTURBED AREAS ABOVE 8.8 FT ELEVATION, EXCLUDING RAILROAD SURFACE, SHALL BE SEEDING WITH NATIVE SPECIES AS SOON AS POSSIBLE AFTER CONSTRUCTION.
2. SITKA SPRUCE (*PICEA SITCHENSIS*) SHALL BE PLANTED AT TOP ELEVATIONS OF TOPOGRAPHIC COMPLEXITY MOUNDS.

NO.	BY	DATE	REVISION DESCRIPTION

GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
 RAILROAD RESTORATION
 AGENCY CREEK - FINAL DESIGN**



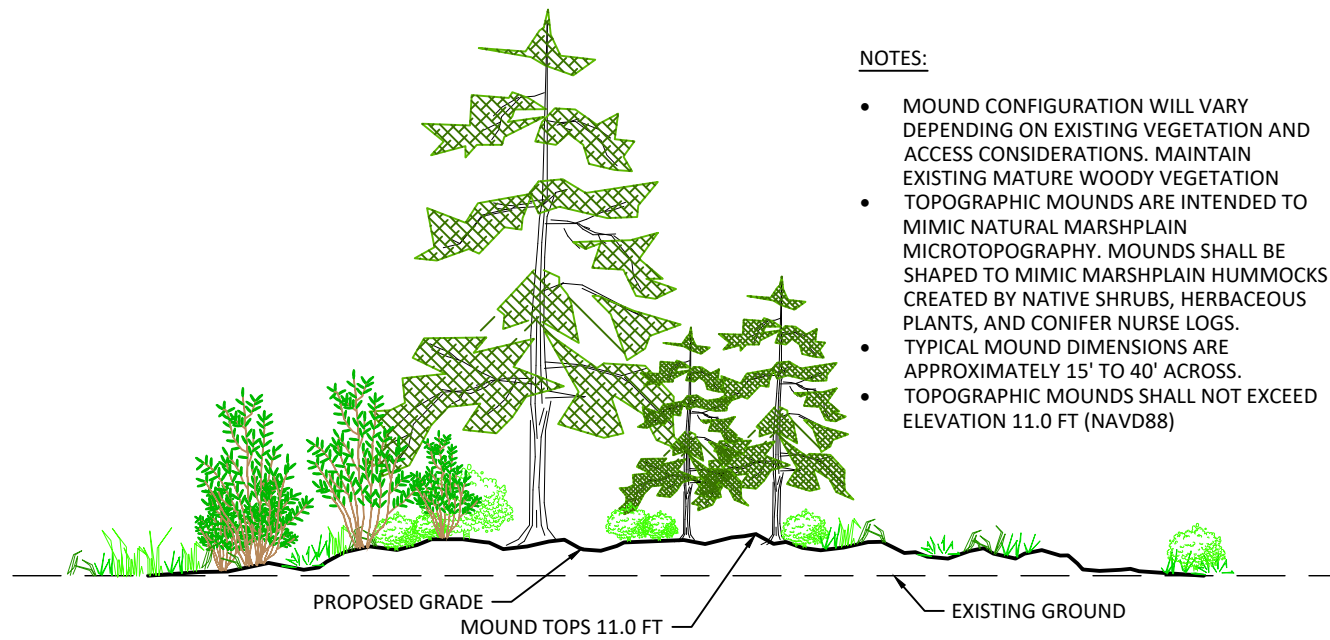
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REVEGETATION PLAN

SHEET

20 OF 40

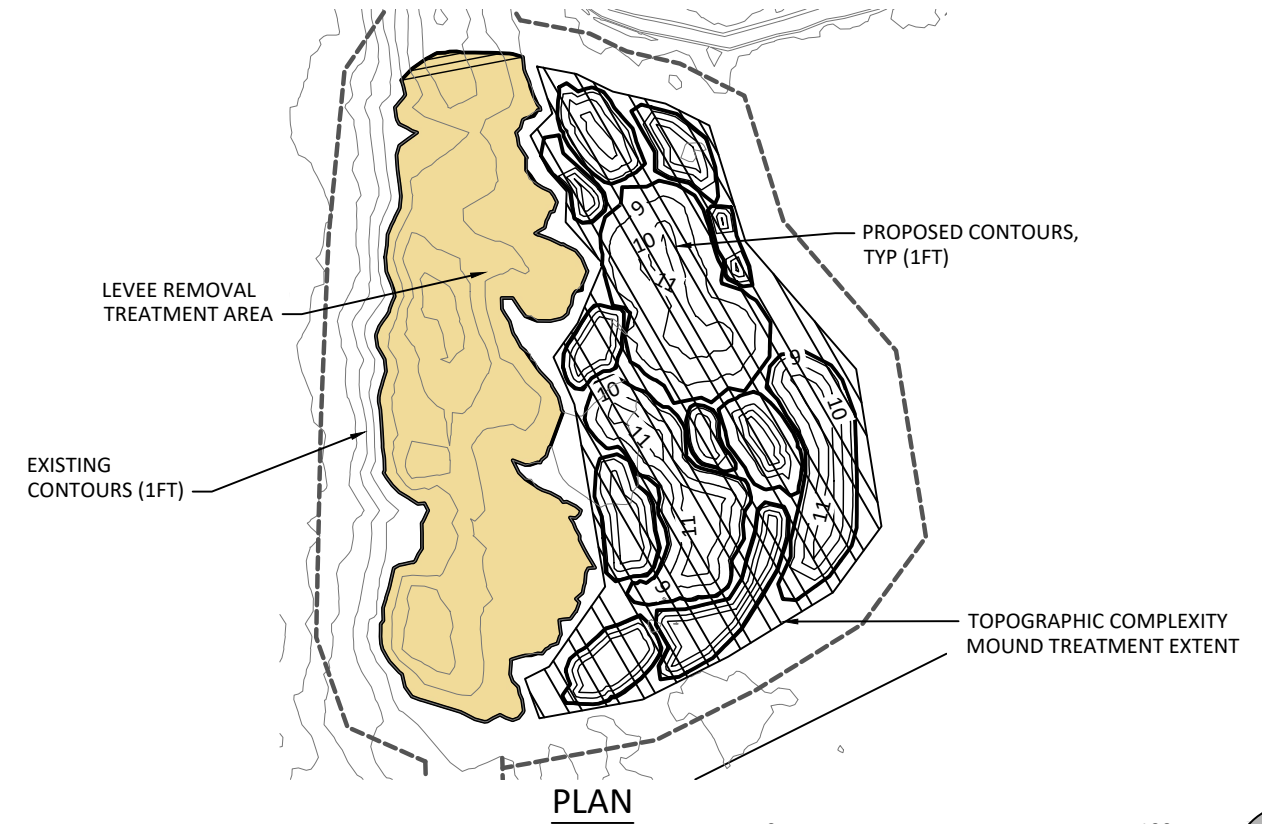
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SECTION

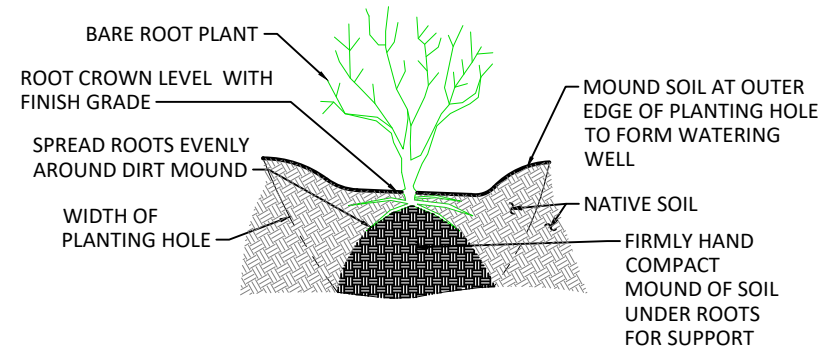
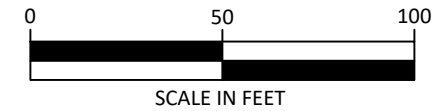
NOTES:

- MOUND CONFIGURATION WILL VARY DEPENDING ON EXISTING VEGETATION AND ACCESS CONSIDERATIONS. MAINTAIN EXISTING MATURE WOODY VEGETATION
- TOPOGRAPHIC MOUNDS ARE INTENDED TO MIMIC NATURAL MARSHPLAIN MICROTOPOGRAPHY. MOUNDS SHALL BE SHAPED TO MIMIC MARSHPLAIN HUMMOCKS CREATED BY NATIVE SHRUBS, HERBACEOUS PLANTS, AND CONIFER NURSE LOGS.
- TYPICAL MOUND DIMENSIONS ARE APPROXIMATELY 15' TO 40' ACROSS.
- TOPOGRAPHIC MOUNDS SHALL NOT EXCEED ELEVATION 11.0 FT (NAVD88)

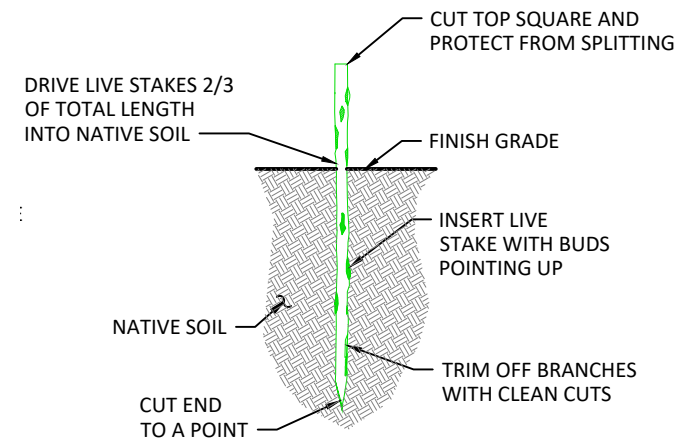


PLAN

1
21 TYPICAL DETAIL - TOPOGRAPHIC COMPLEXITY MOUND
NOT TO SCALE



2
21 TYPICAL DETAIL - BARE ROOT PLANT
NOT TO SCALE



3
21 TYPICAL DETAIL - LIVE CUTTING
NOT TO SCALE

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COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



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REVEGETATION DETAILS (1 OF 2)

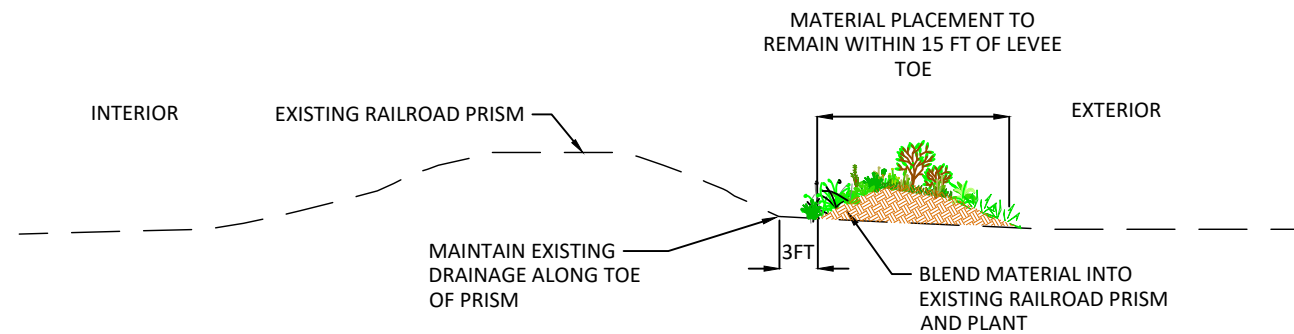
SHEET

21 OF 40

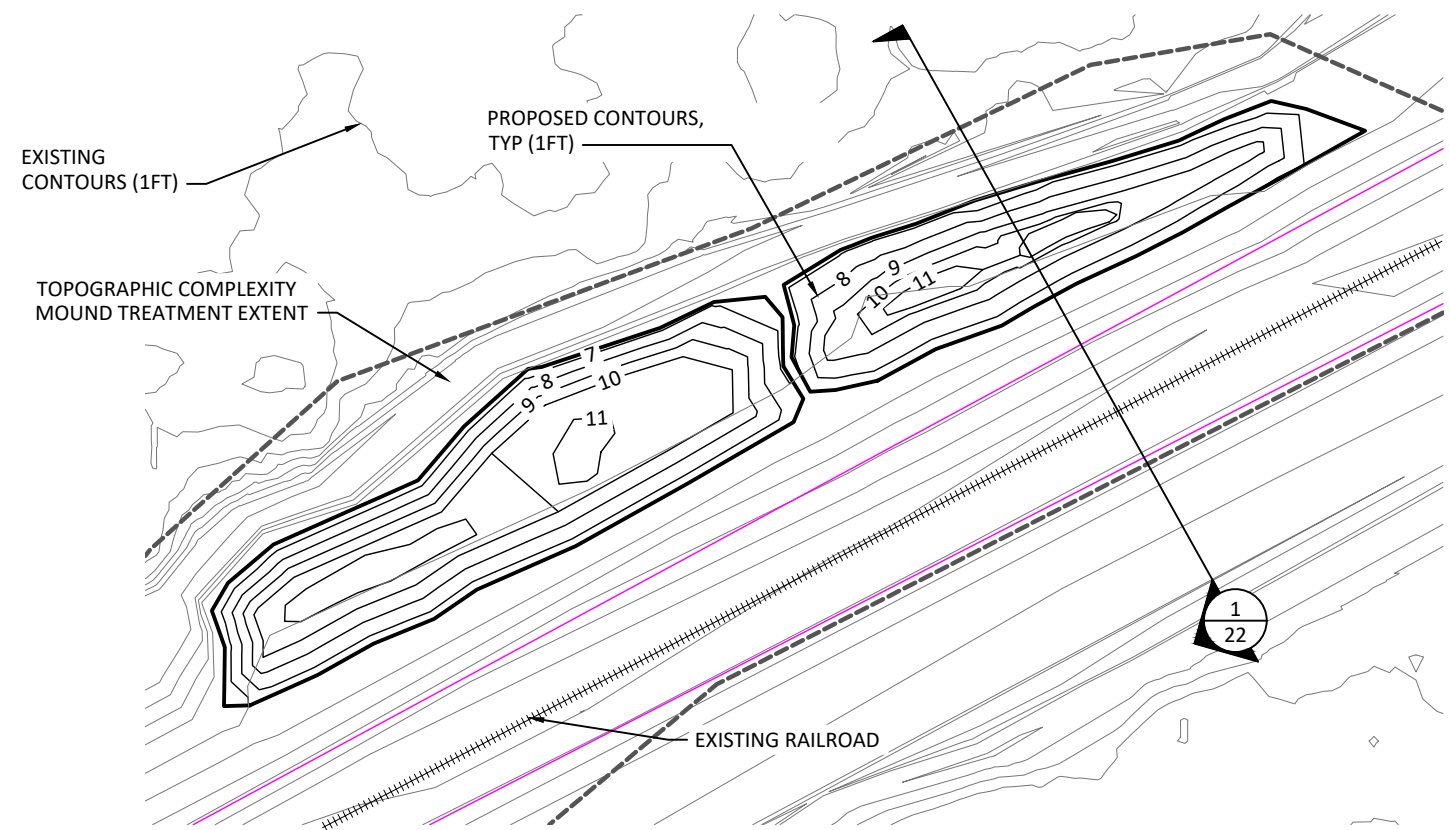
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NOTES:

- TOPOGRAPHIC COMPLEXITY ZONE CONFIGURATION TO VARY DEPENDING ON EXISTING VEGETATION AND ACCESS CONSIDERATIONS. TOPOGRAPHIC COMPLEXITY ZONE IS INTENDED TO MIMIC NATURAL MICROTOPOGRAPHY AND HUMMOCKS CREATED BY VEGETATION AND TO PROVIDE A VEGETATION BUFFER BETWEEN MARSHPLAIN AND THE RAILROAD PRISM.
- FILL MATERIAL PLACEMENT SHALL START A MINIMUM OF 3 FT FROM THE TOE OF THE EXISTING RAILROAD EMBANKMENT, PER THE G&W PUBLIC PROJECT MANUAL.
- LEVEE SHOULDER VEGETATION COMPLEXITY ZONE SHALL NOT EXCEED ELEVATION 11.0 FT (NAVD88).



1
22 TYPICAL DETAIL - PROPOSED LEVEE SHOULDER VEGETATION COMPLEXITY ZONE
NOT TO SCALE



NO.	BY	DATE	REVISION DESCRIPTION

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APPROVED	DATE	PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**



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REVEGETATION DETAILS (2 OF 2)

SHEET

22 OF 40

AGENCY CREEK AND WARREN SLOUGH STANDARD PLANS DECEMBER 7, 2022

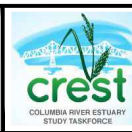
80% SUBMITTAL
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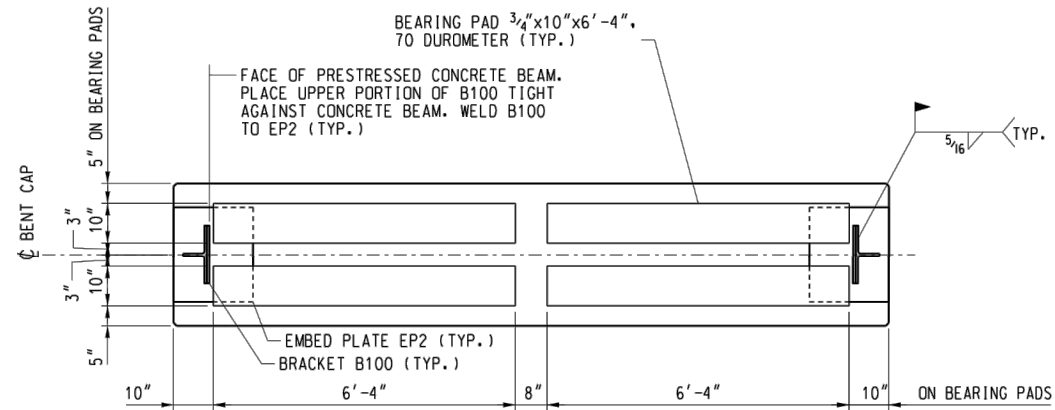
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RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



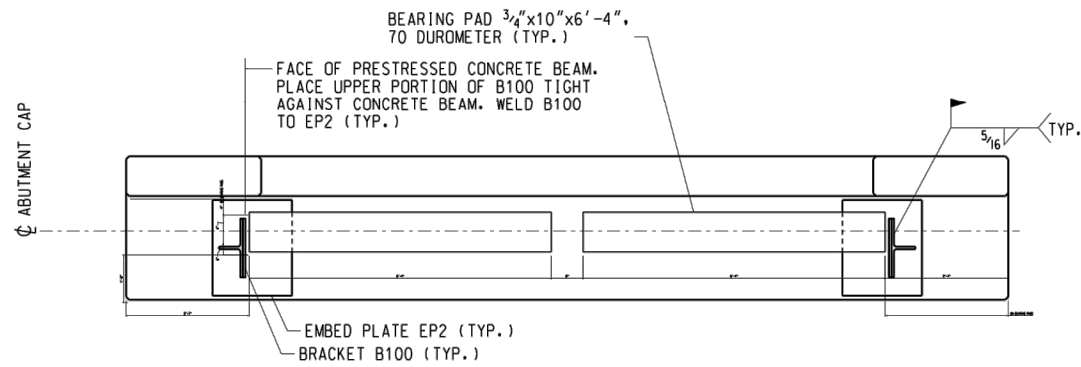
BRIDGE STANDARDS - TITLE
PAGE

SHEET
23 OF 40

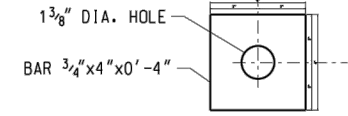
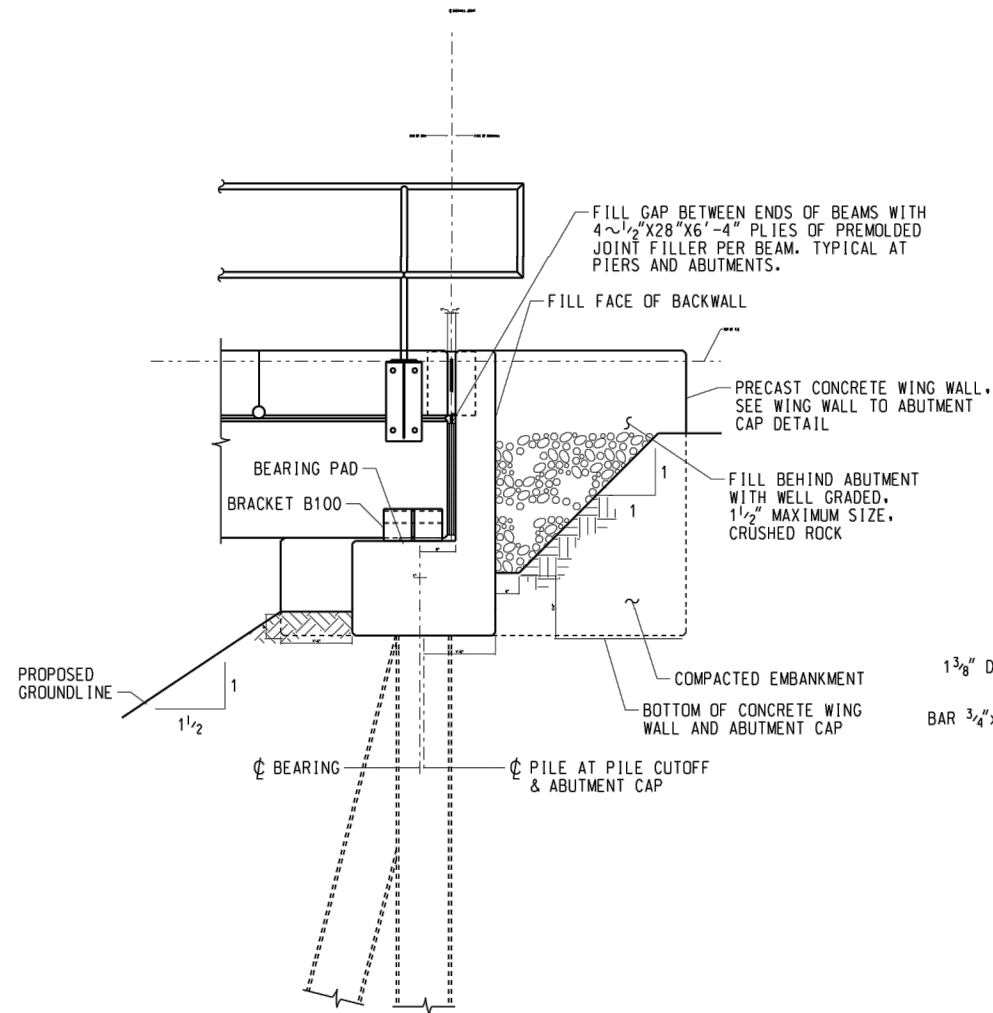
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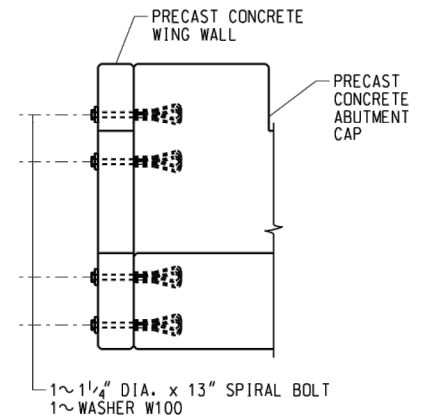
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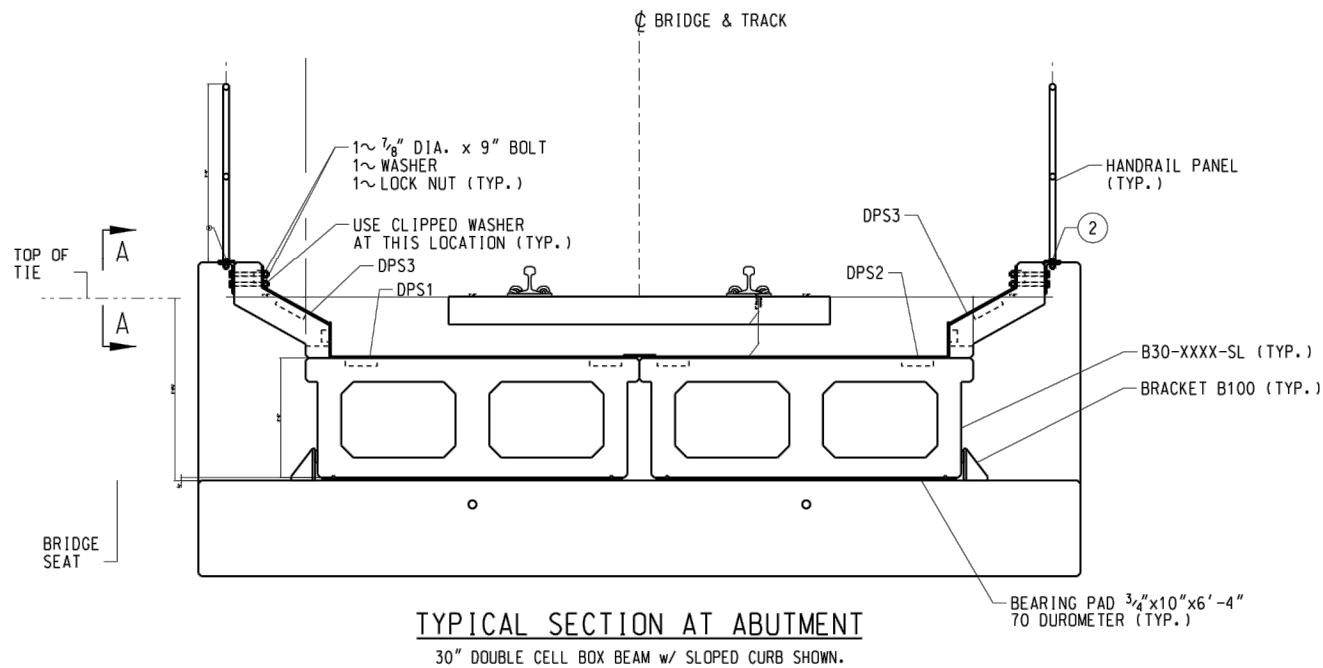
TYPE 1 - ABUTMENT



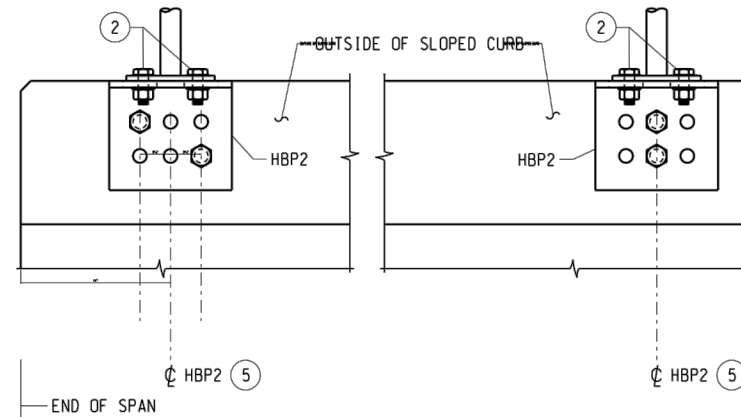
WASHER W100
GALVANIZE AFTER FABRICATION



WING WALL TO ABUTMENT CAP DETAIL



TYPICAL SECTION AT ABUTMENT



VIEW A-A

2 FASTEN HANDRAIL PANEL TO BRACKET WITH:
2~ $\frac{3}{4}$ " DIA. x 2 1/4" BOLTS
4~WASHERS ($\frac{13}{16}$ " I.D. x 1 7/16" O.D.)
2~LOCK NUTS, CENTER LOCKING, ZINC PLATES

5 FASTEN HBP2 BRACKET TO CURB WITH:
2~ $\frac{1}{8}$ " DIA. x 9" BOLT
2~WASHERS ($\frac{15}{16}$ " I.D. x 1 3/4" O.D.)
2~LOCK NUT, CENTER LOCKING, ZINC PLATED

80% SUBMITTAL
NOT FOR CONSTRUCTION

NO.	BY	DATE	REVISION DESCRIPTION

GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
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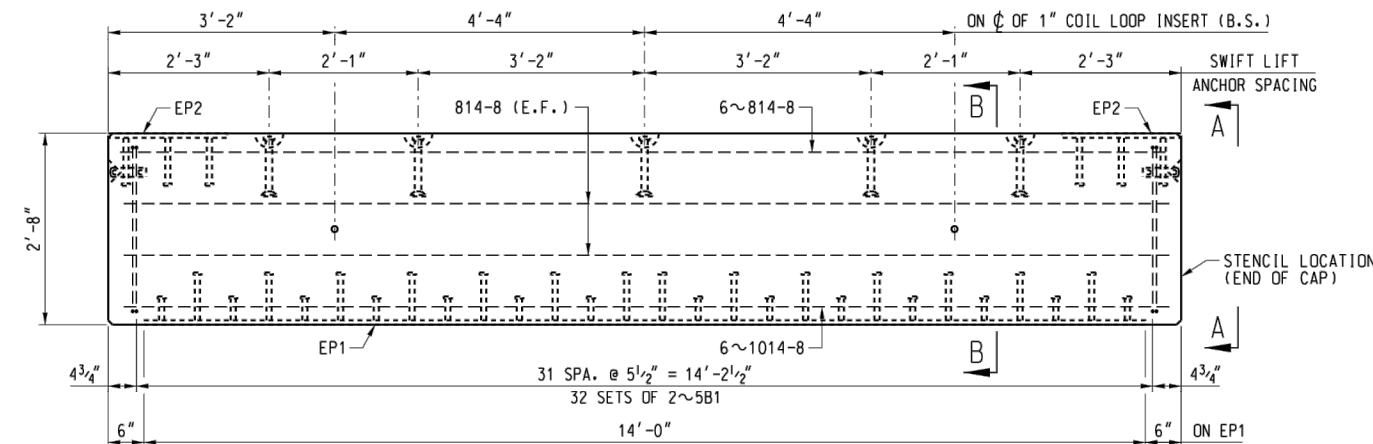
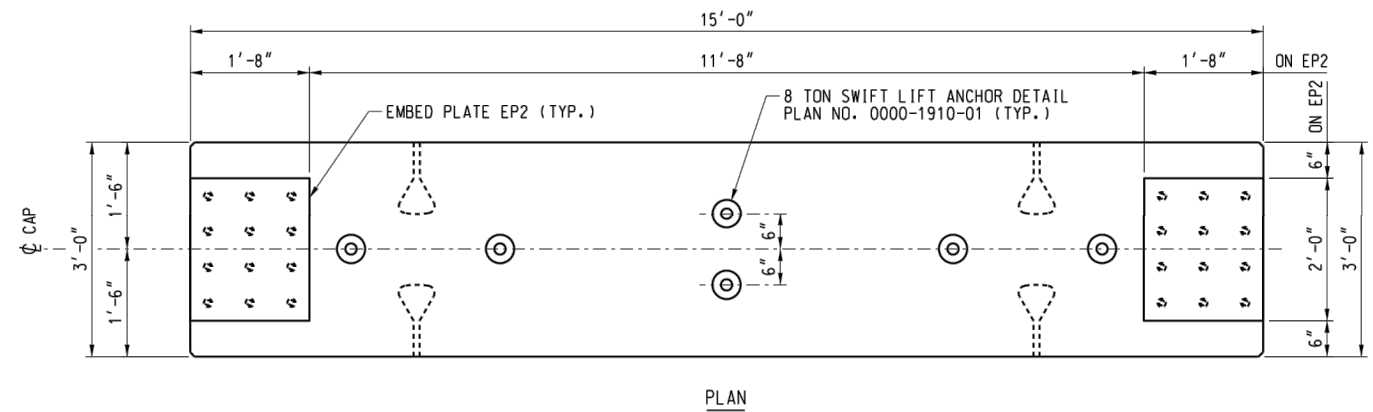
COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



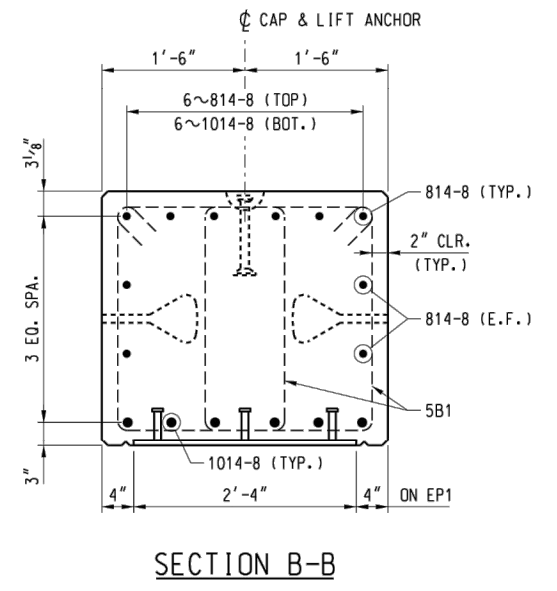
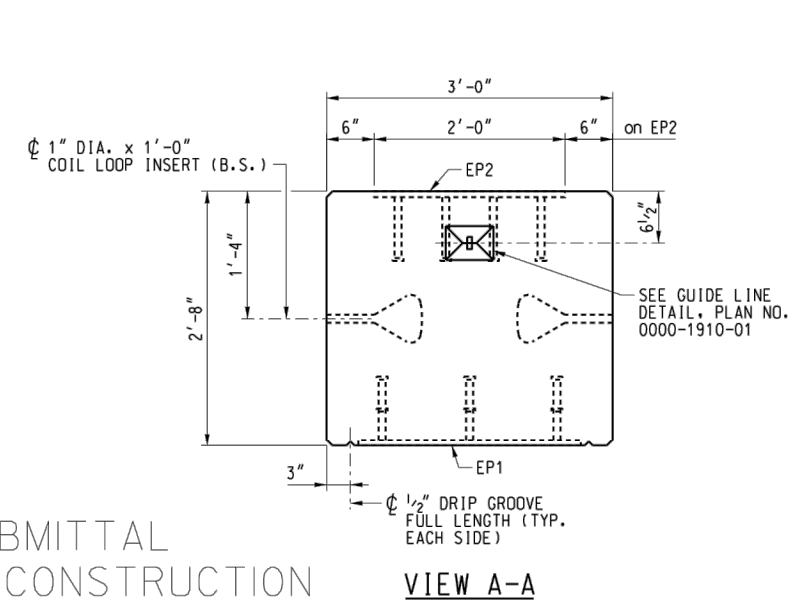
BRIDGE STANDARDS - TYPICAL
ELEVATIONS

SHEET
25 OF 40

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PRECAST BENT CAP C00-1
EST. WEIGHT = 20,060 LBS.



80% SUBMITTAL
NOT FOR CONSTRUCTION

LIST OF REINFORCING BARS					
MARK	SIZE	TYPE	A	B	LENGTH
5B1	#5	B	1'-9"	2'-4"	9'-1"
814-8	#8	STR.	-	-	14'-8"
1014-8	#10	STR.	-	-	14'-8"

1. SEE PLAN NO. 0000-xxxx-xx FOR BENDING DIAGRAM.

BILL OF MATERIAL		
QTY.	UNIT	DESCRIPTION
1	EA.	EP1 (SEE DETAIL, PLAN NO. 0000-1910-03)
2	EA.	EP2 (SEE DETAIL, PLAN NO. 0000-1910-03)
6	EA.	8 TON SWIFT LIFT ANCHOR
2	EA.	8" 4 TON T-BAR ANCHOR
4	EA.	1" DIA. x 1'-0" COIL LOOP INSERT

NO.	BY	DATE	REVISION DESCRIPTION

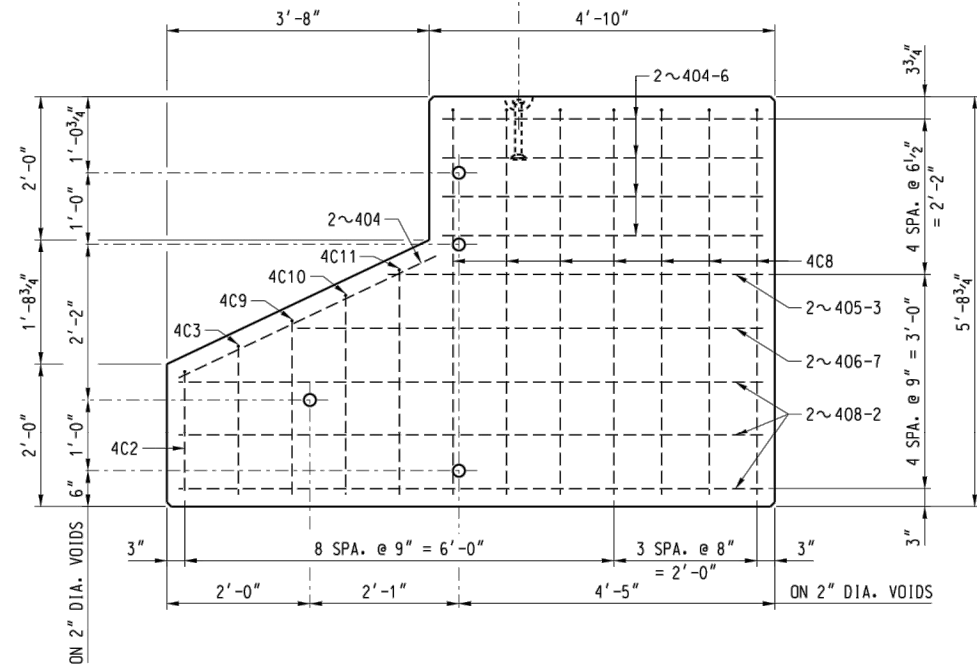
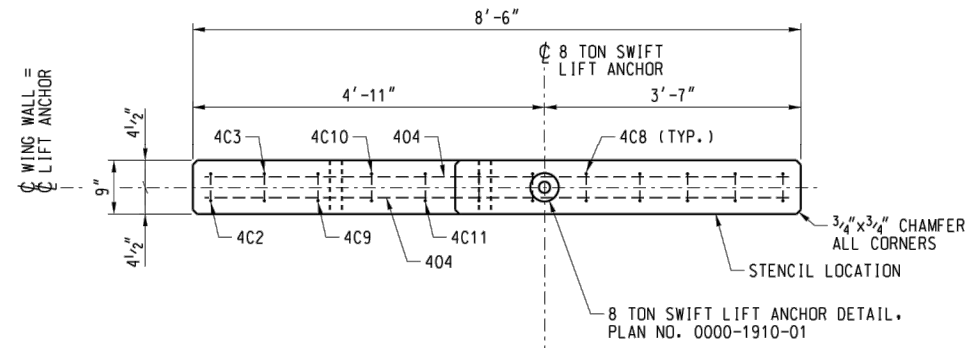
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

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RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



BRIDGE STANDARDS - BENT
CAP

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PRECAST WING WALL W20-T
EST. WEIGHT = 4,470 LBS.

LIST OF REINFORCING BARS

MARK	SIZE	TYPE	A	B	LENGTH
4C2	#4	C	5"	1'-9"	3'-9"
4C3	#4	C	5"	2'-1 1/4"	4'-3"
4C8	#4	C	5"	5'-4 3/4"	11'-1"
4C9	#4	C	5"	2'-5 1/2"	5'-0"
4C10	#4	C	5"	2'-9 3/4"	5'-7"
4C11	#4	C	5"	3'-2"	6'-3"
4C12	#4	C	5"	4'-9 3/4"	9'-11"
404	#4	STR.	-	-	4'-0"
404-6	#4	STR.	-	-	4'-6"
404-10	#4	STR.	-	-	4'-10"
405-3	#4	STR.	-	-	5'-3"
406-7	#4	STR.	-	-	6'-7"
408-2	#4	STR.	-	-	8'-2"

1. SEE PLAN NO. 0000-1910-07 FOR BENDING DIAGRAM.

BILL OF MATERIAL

W20-T	UNIT	DESCRIPTION
1	EA.	8 TON SWIFT LIFT ANCHOR

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NOT FOR CONSTRUCTION

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**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**

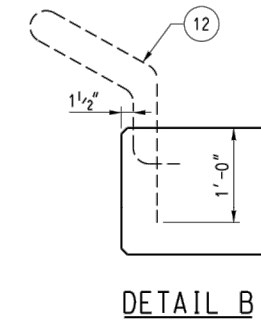
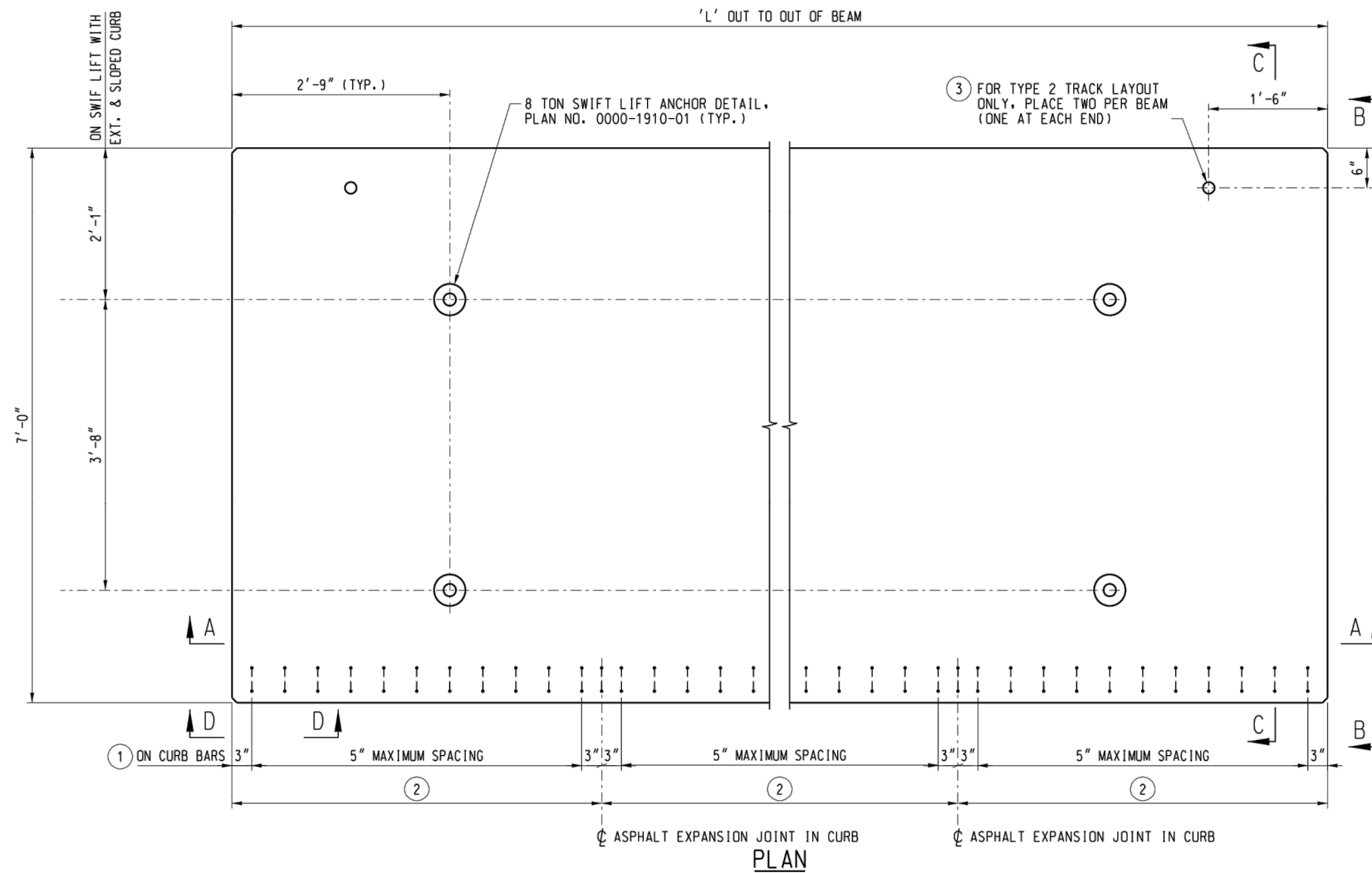


**BRIDGE STANDARDS - 20 INCH
WING WALL**

SHEET

28 OF 40

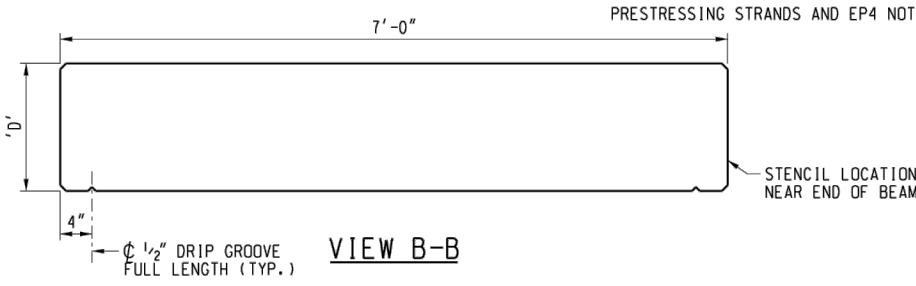
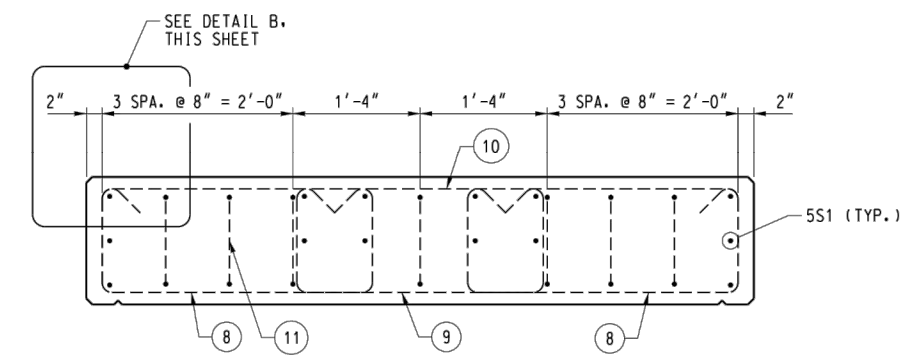
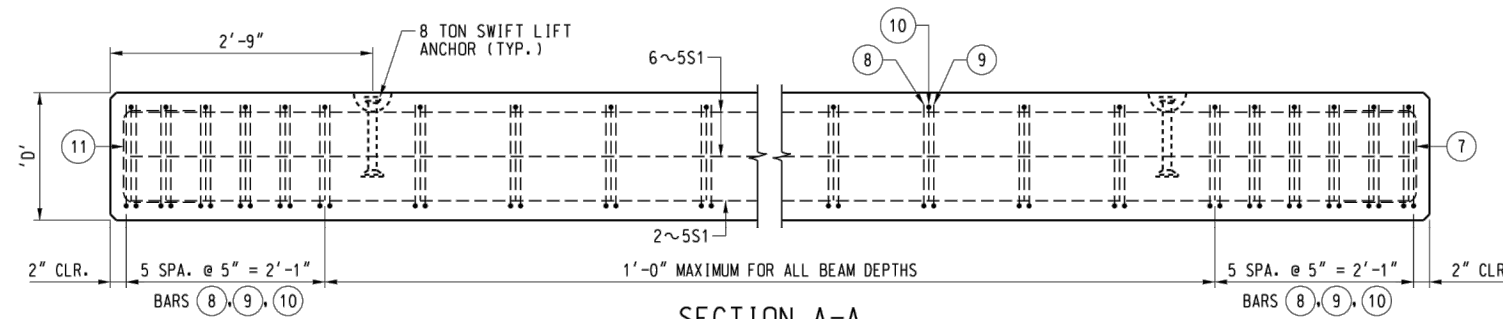
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AVAILABLE SPAN LENGTHS	
20" BEAM: 16'-0" TO 22'-0"	
DESIGN LOADS	
DEAD:	TRACK, FASTENERS, ETC. 200
	BALLAST 4,065
	CURB, WALK & HANDRAIL 560
	BEAMS 3,500
	TOTAL (LBS./FT. OF TRACK) 8,325
LIVE:	SEE GENERAL NOTES, PLAN NO. 0000-1000-02.
IMPACT:	SEE GENERAL NOTES, PLAN NO. 0000-1000-02.
EST. LIFTING WEIGHT	
20" BEAM DEPTH: 1,820 PLF (NO CURB)	

MINIMUM CONCRETE BEAM COMPRESSIVE STRENGTHS			
SPAN LENGTH (ft)	BEAM DEPTH (in)	AT TRANSFER (psi)	AT 28 DAYS (psi)
16 - 22	20	4,500	7,000

- ① ADJUST AS REQUIRED TO MISS OTHER REINFORCEMENT AND FERRULE INSERTS.
- ② SPACING OF ASPHALT EXPANSION JOINT IN CURB. DIMENSION TO EQUAL 'L' DIVIDED BY NUMBER OF CURB SEGMENTS EXCEPT WHERE ADJUSTMENT IS REQUIRED TO MISS REINFORCEMENT AND FERRULE INSERTS. SEE PLAN NO. 0000-1000-05 FOR NUMBER OF CURB SEGMENTS.
- ⑧ BAR SA2 FOR 20" BEAM DEPTH
- ⑨ BAR SA4 FOR 20" BEAM DEPTH
- ⑩ BAR 5E4 FOR 20" BEAM DEPTH
- ⑪ BAR 5E2 FOR 20" BEAM DEPTH
- ⑫ SEE CURB & WALK STANDARD FOR CURB DETAIL. BAR 4L1 FOR SLOPED CURB.



SECTION C-C
ALL STEEL IS TO HAVE AT LEAST 1 1/2" COVER UNLESS OTHERWISE NOTED. SEE PLAN NO. 0000-1210-02 FOR LOCATION OF PRESTRESSING STRANDS.

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MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



BRIDGE STANDARDS - 20 INCH
SLAB BEAM

SHEET
30 OF 40

NO.	BY	DATE	REVISION DESCRIPTION

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LIST OF REINFORCING BARS

MARK	SIZE	TYPE	A	B	LENGTH
4C26	#4	C	4"	2'-5 1/2"	5'-1"
4C27	#4	C	4"	3'-0 1/2"	6'-3"
4L1	#4	L	-	-	5'-10"
5A1	#5	A	2'-10"	1'-1"	5'-9"
5A2	#5	A	2'-10"	1'-4"	6'-5"
5A3	#5	A	2'-7"	1'-1"	5'-7"
5A4	#5	A	2'-7"	1'-5"	6'-3"
5E1	#5	E	11 1/2"	9"	2'-6"
5E2	#5	E	1'-3 1/2"	9"	2'-10"
5E3	#5	E	6'-8"	1'-0"	8'-8"
5E4	#5	E	6'-8"	1'-4"	9'-4"
5S1	#5	STR.	-	-	(L-5")

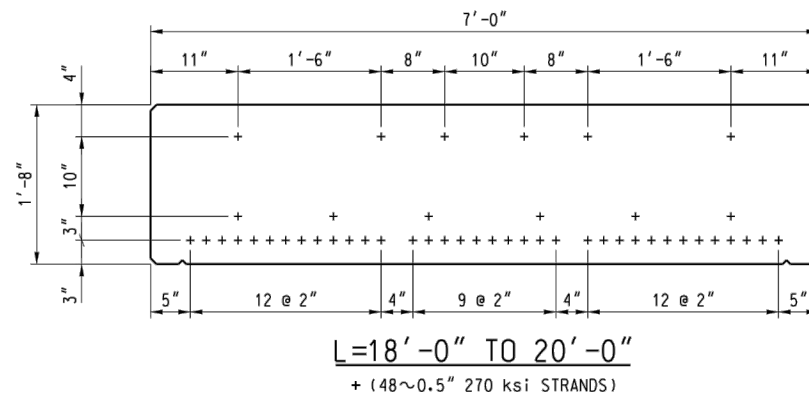
- 'L' IS THE LENGTH OF THE PRESTRESSED CONCRETE BEAM. LENGTH OF BENT BARS ARE NORMAL.
- SEE PLAN NO. 0000-1910-07 FOR BENDING DIAGRAM.

PRESTRESSING STRANDS

SPAN LENGTH (ft.)	BEAM DEPTH (in.)	NO. STRANDS	INITIAL PULL (k)	PS CENTROID FROM BOT. (in.)
14 - 16	16	42	1,302	4.380
16 - 18	20	38	1,178	4.840
18 - 20	20	48	1,488	5.000
20 - 22	20	58	1,798	5.070

NOTES:

- PRESTRESSING STRANDS SHALL BE 0.5 INCH DIAMETER, SEVEN WIRE, UNCOATED, LOW RELAXATION STRAND WHICH IS IN ACCORDANCE WITH THE REQUIREMENTS SPECIFIED IN ASTM A416. THE STRAND SHALL HAVE AN ULTIMATE TENSILE STRENGTH OF 270 ksi. THE INITIAL PRESTRESS SHALL BE 31,000 LBS. PER STRAND UNLESS NOTED OTHERWISE.



80% SUBMITTAL
NOT FOR CONSTRUCTION

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COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN

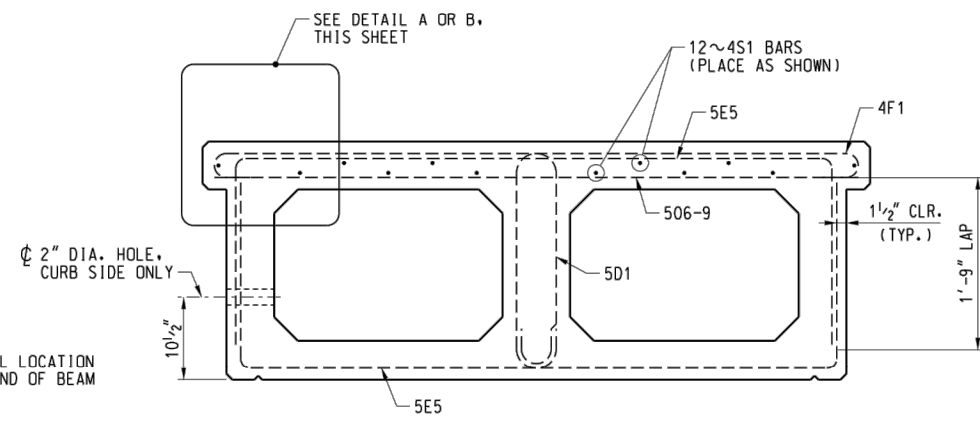
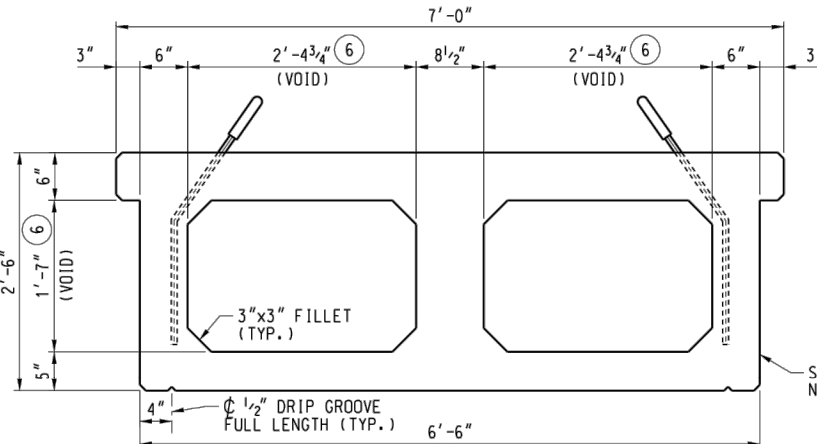
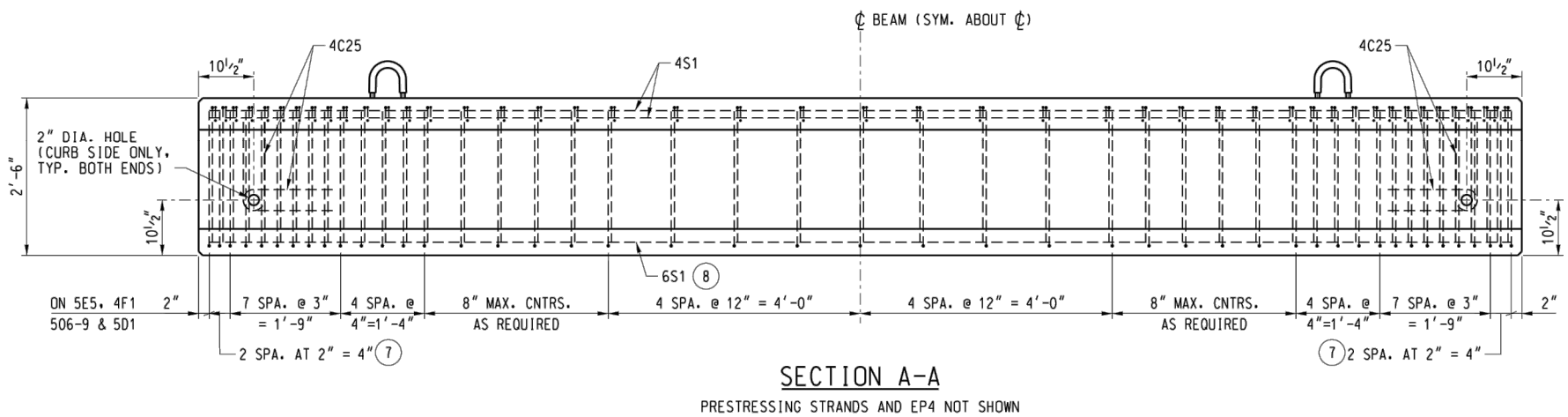
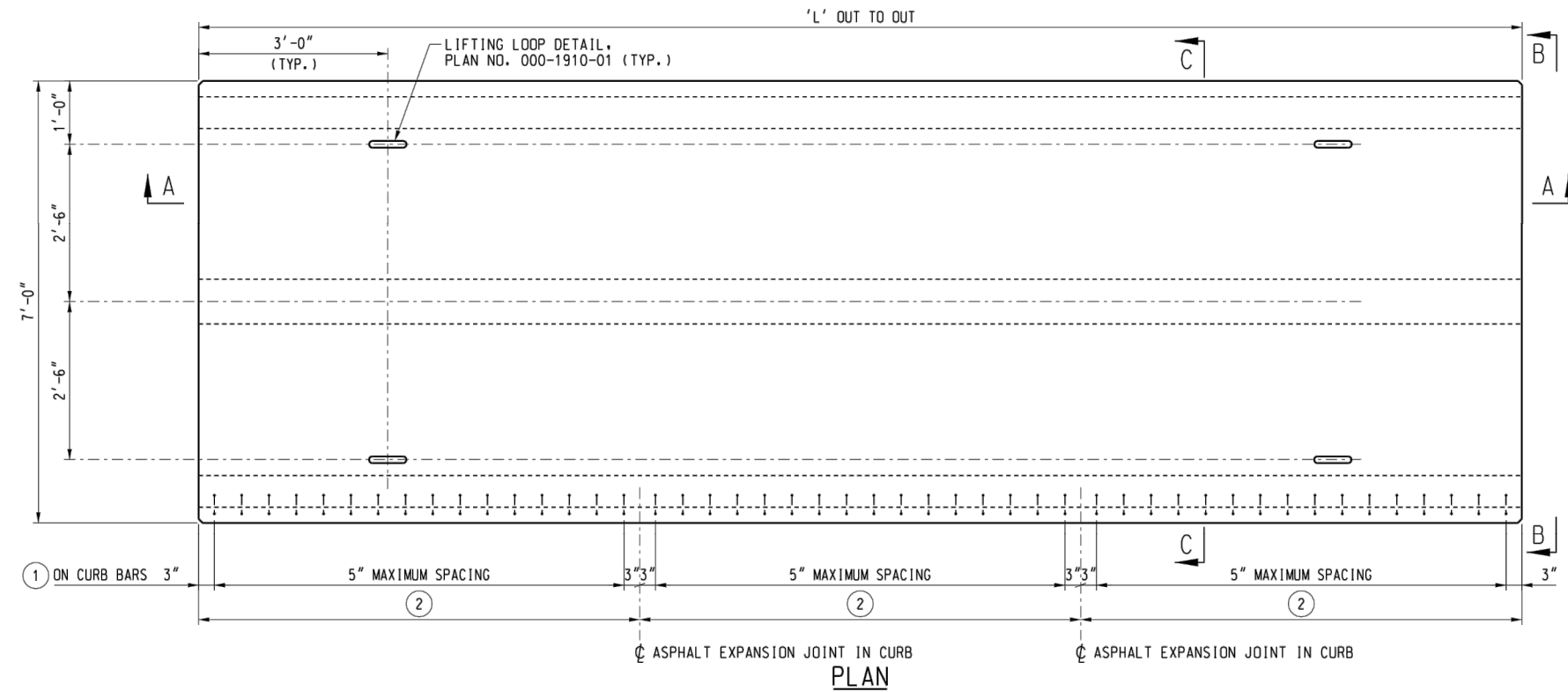


BRIDGE STANDARDS - 20 INCH
SLAB BEAM

SHEET

31 OF 40

Z:\ClientFiles\0-1-Railroad-AgencyCkWarrenSlouth_CREEST_190220\Drawings\LEI_AGENCY_CREEK_LEI_RAILROAD_Agency_D.dwg - ashear - 2/19/24



AVAILABLE SPAN LENGTHS

20'-0" TO 36'-0"

DESIGN LOADS

DEAD:	TRACK, FASTENERS, ETC.	200
	BALLAST	4,065
	CURB, WALK & HANDRAIL	560
	BEAMS	2,825
	TOTAL (LBS./FT. OF TRACK)	7,650

LIVE: SEE GENERAL NOTES, PLAN NO. 0000-1000-02.
 IMPACT: SEE GENERAL NOTES, PLAN NO. 0000-1000-02.

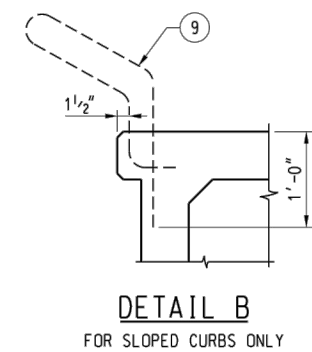
EST. LIFTING WEIGHT

30" BEAM DEPTH: 1,430 PLF (NO CURB)

MINIMUM CONCRETE BEAM COMPRESSIVE STRENGTHS

SPAN LENGTH (ft)	BEAM DEPTH (in)	BEAM TYPE	AT TRANSFER (psi)	AT 28 DAYS (psi)
20 - 30	30	I	4,500	6,000
34 - 36	30	I	5,500	7,000

- 1 ADJUST AS REQUIRED TO MISS OTHER REINFORCEMENT AND FERRULE INSERTS.
- 2 SPACING OF ASPHALT EXPANSION JOINT IN CURB. DIMENSION TO EQUAL 'L' DIVIDED BY NUMBER OF CURB SEGMENTS EXCEPT WHERE ADJUSTMENT IS REQUIRED TO MISS REINFORCEMENT AND FERRULE INSERTS. SEE PLAN NO. 0000-1000-02 FOR NUMBER OF CURB SEGMENTS.
- 3 CAST-IN-PLACE FERRULE INSERTS. FOR 7/8" DIA. BOLT MINIMUM SHEAR = 3,000 LBS. FACTOR OF SAFETY 3 TO 1 AND A MINIMUM TENSION = 3,660 LBS. 4 INSERTS PER WALK BRACKET LOCATION. FURNISH WITH 7/8" DIA. BOLTS AND WASHERS. ALL ITEMS SHALL BE GALVANIZED.
- 4 ϕ FERRULE INSERTS. LOCATION AND SPACING PER CURB AND WALK STANDARD.
- 5 VOID DIMENSIONS SHOWN ARE MAXIMUM AND MUST NOT BE EXCEEDED AT ANY POINT INCLUDING SPLICES OF VOID FORM.
- 6 INCREASE END STIRRUP GROUP TO 6 SPACES AT 2" = 1'-0" FOR L > 34'-0". ALL OTHER SPACINGS REMAIN THE SAME.
- 7 SEE PRESTRESSING STRAND PATTERN SHEETS FOR LOCATION OF 6S1 BARS.
- 8 SEE CURB & WALK STANDARD FOR CURB DETAIL. BAR 4L1 FOR SLOPED CURB.



80% SUBMITTAL
NOT FOR CONSTRUCTION

GS	MC, CA, MB	MB
DRAWN	DESIGNED	CHECKED
MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



BRIDGE STANDARDS - 30 INCH
DOUBLE CELL BOX BEAMS

Z:\ClientFiles\0-T-Railroad-AgencyC\Warren\Drawings\CREST_190220\Drawings\I\Railroad-AgencyCreek-D\I\RAILROAD_AgencyC.dwg - rshar - 2/19/24

LIST OF REINFORCING BARS

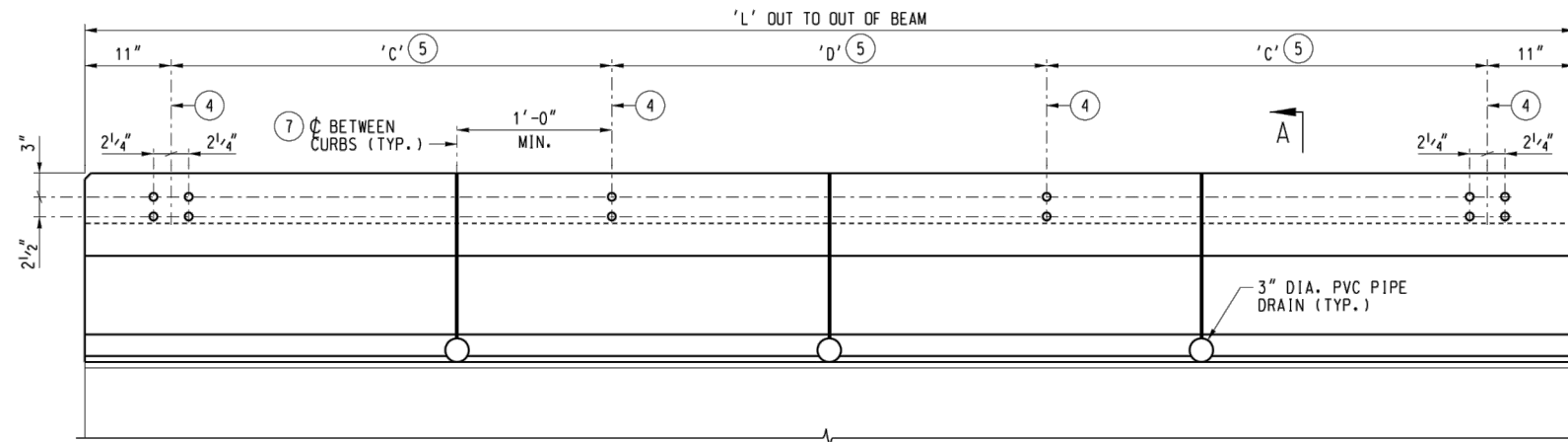
MARK	SIZE	TYPE	A	B	LENGTH
4K1	#4	K	-	-	4'-7"
4S1	#4	STR.	-	-	(6)
5J1	#5	J	2'-9"	1'-3"	4'-0"

- 'L' IS THE LENGTH OF THE PRESTRESSED CONCRETE BEAM.
- SEE PLAN NO. XXXX-1910-07 FOR BENDING DIAGRAM.

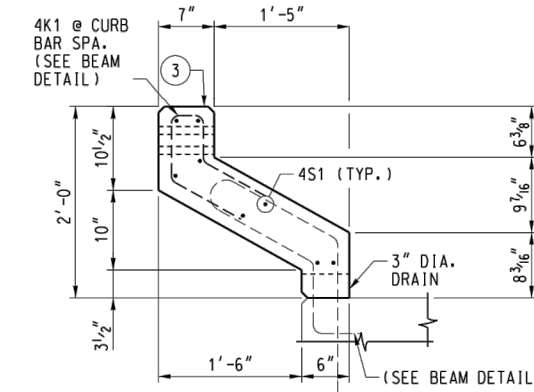
EST. WEIGHT (PLF)

CURB TYPE	WEIGHT PER LINEAR FOOT OF ONE CURB
24" SLOPED	250

- CAST-IN-PLACE FERRULE INSERTS, FOR 7/8" DIA. BOLT MINIMUM SHEAR = 3,000 LBS. FACTOR OF SAFETY 3 TO 1 AND A MINIMUM TENSION = 3,660 LBS. 4 INSERTS PER WALK BRACKET LOCATION, FURNISH WITH 7/8" DIA. BOLTS AND WASHERS. ALL ITEMS SHALL BE GALVANIZED.
- COIL LOOP INSERTS ARE TO BE SINGLE FLARED TYPE B-18, 1" DIA. x 12" AND HAVE A SAFE WORKING LOAD 4,750 LBS. WITH A FACTOR OF SAFETY OF 4 TO 1. THE INSERTS ARE TO BE COMPLETELY RECESSED WITH SPIRAL BOLTS FURNISHED IN THE INSERT.
- 3"x6" I.D. PLATE IS TO BE EMBEDDED FLUSH IN TOP OF CURB AND CENTERED 6" FROM END OF CURB. PLATE SHALL CONTAIN THE FOLLOWING INFORMATION IN 1/2" LETTERING: ITEM NAME, LENGTH, WEIGHT, DATE MANUFACTURED AND NAME OF MANUFACTURER.
- 1" DIAMETER HOLES FOR HANDRAIL POST.
- SEE TABLE ON PLAN NO. 0000-1000-02 FOR SPACING.
- LENGTH OF BAR EQUAL TO LENGTH OF CURB SEGMENT MINUS 4".
- SEE NOTES AND TABLE ON PLAN NO. 0000-1000-02 FOR NUMBER OF EQUAL LENGTH CURBS PER BEAM LENGTH.

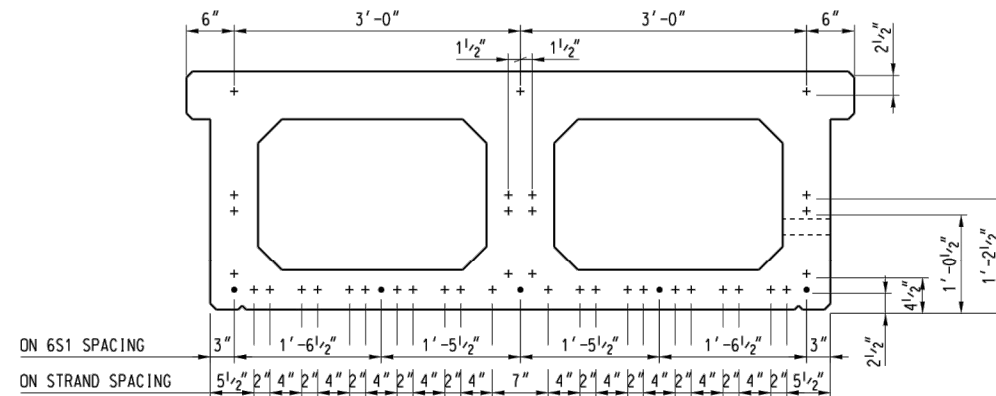


ELEVATION - SLOPED CURB



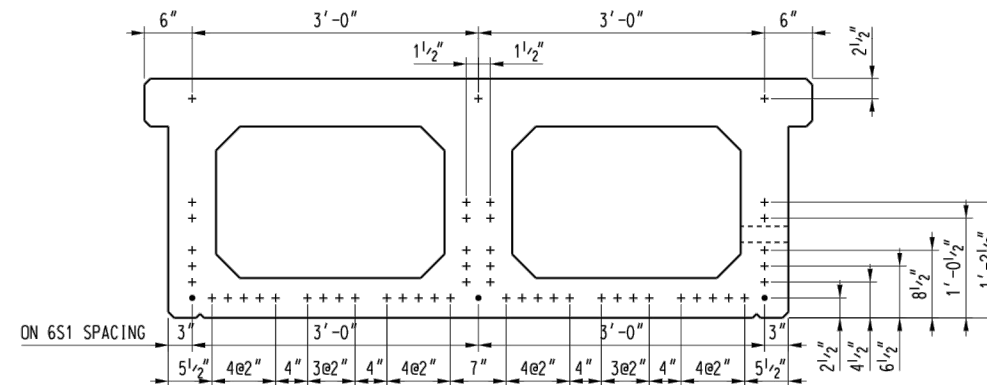
SECTION A-A

24" CURB



30" DOUBLE CELL BOX BEAM 30' PRESTRESSING STRAND PATTERN - TYPE I

+ (37~0.6" 270 KSI STRANDS)
• (5~#6 REINFORCING BARS)



30" DOUBLE CELL BOX BEAM 36' PRESTRESSING STRAND PATTERN - TYPE I

+ (51~0.6" 270 KSI STRANDS)
• (3~#6 REINFORCING BARS)

PRESTRESSING STRANDS					LIST OF REINFORCING BARS					
SPAN LENGTH (ft.)	BEAM DEPTH (in.)	NO. STRANDS	INITIAL PULL (k)	PS CENTROID FROM BOT. (in.)	MARK	SIZE	TYPE	A	B	LENGTH
28 - 30	30	37	1,605.8	7.120	4C25	#4	C	4"	1'-5"	3'-2"
34 - 36	30	51	2,213.4	6.640	4F1*	#4	F	6'-9"	3"	7'-7"
					4H1	#4	H	4"	2'-5 1/2"	5'-1"
					4H2	#4	H	4"	3'-0 1/2"	6'-3"
					4L1	#4	L	-	-	5'-10"
					4S1	#4	STR.	-	-	L-4"
					5D1	#5	D	5"	2'-3"	6'-1"
					5E5	#5	E	6'-3"	2'-0"	10'-3"
					506-9	#5	STR.	-	-	6'-9"
					6S1	#6	STR.	-	-	L-4"

- 'L' IS THE LENGTH OF THE PRESTRESSED CONCRETE BEAM. LENGTH OF BENT BARS ARE NORMAL.
 - SEE PLAN NO. 0000-1910-07 FOR BENDING DIAGRAM.
- * NON-STANDARD HOOK.

NOTES:

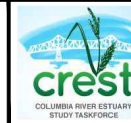
- PRESTRESSING STRANDS SHALL BE 0.6 INCH DIAMETER, SEVEN WIRE, UNCOATED, LOW RELAXATION STRAND WHICH IS IN ACCORDANCE WITH THE REQUIREMENTS SPECIFIED IN ASTM A416. THE STRAND SHALL HAVE AN ULTIMATE TENSILE STRENGTH OF 270 ksi. THE INITIAL PRESTRESS SHALL BE 43,400 LBS. PER STRAND UNLESS NOTED OTHERWISE.
- TYPE I BEAMS HAVE BEEN DESIGNED TO ACCOMMODATE A MAXIMUM OFFSET BETWEEN THE CENTERLINE OF TRACK AND THE CENTER OF THE LONGITUDINAL JOINT BETWEEN BEAMS OF 6 INCHES. TYPE I BEAMS SHALL BE SUPPLIED WITH CURB.

80% SUBMITTAL
NOT FOR CONSTRUCTION

NO.	BY	DATE	REVISION DESCRIPTION

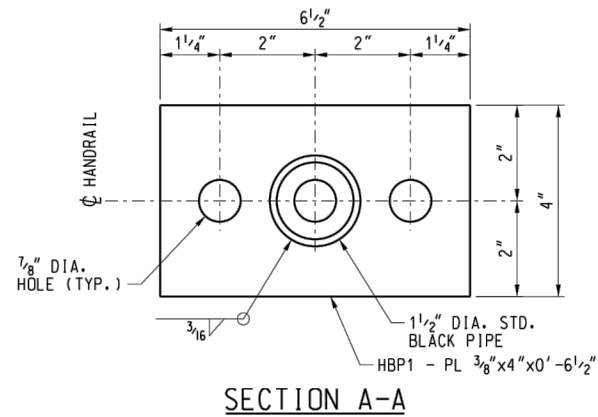
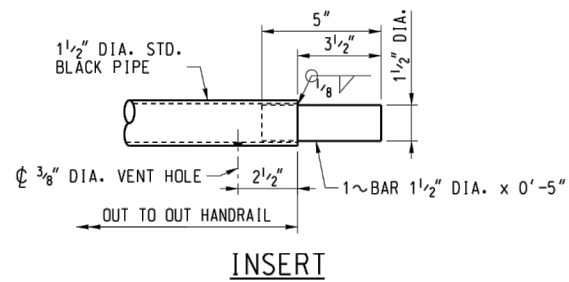
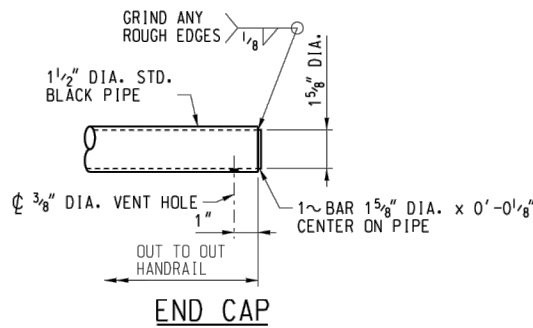
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

**COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN**



**BRIDGE STANDARDS - SLOPED
CURB AND STRAND PATTERN**

SHEET
33 OF 40



NOTES:

1. V = 3/8" DIA. DRILLED VENT HOLE 1" FROM JOINT.

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GS	MC, CA, MB	MB
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MC	02/2024	190220
APPROVED	DATE	PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN

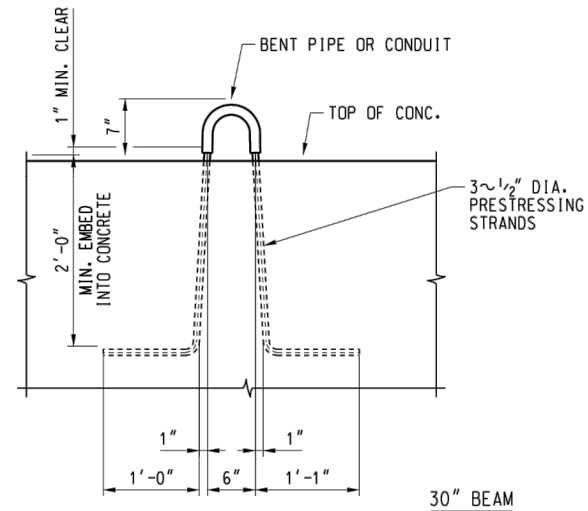


BRIDGE STANDARDS - HAND
RAIL STANDARDS

SHEET

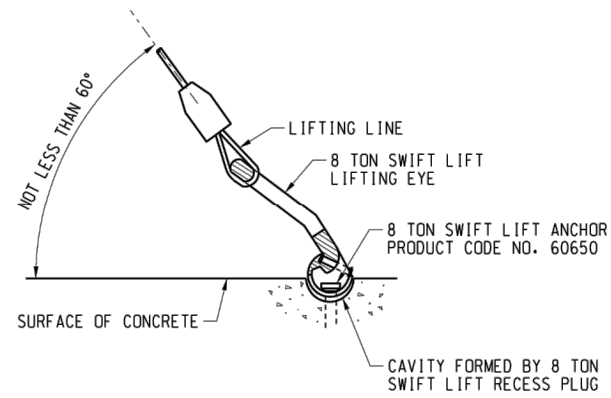
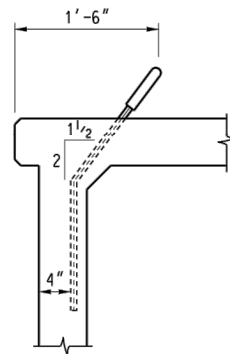
34 OF 40

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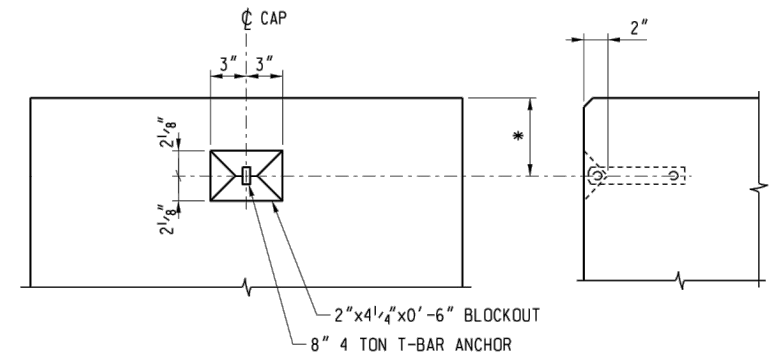
LIFTING LOOP DETAIL

FABRICATOR IS RESPONSIBLE FOR DEVELOPING LIFTING LOOP ANCHORAGE DETAIL TO PROVIDE SAFETY FACTOR OF 4 ON WORKING LOAD. DETAIL SHALL BE PROOF TESTED WITH TEST RESULTS KEPT ON FILE BY FABRICATOR AND AVAILABLE FOR INSPECTION BY THE RAILROAD.



SWIFT LIFT DETAIL

8 TON SWIFT LIFT RECESS PLUGS, ANCHORS AND LIFT. EYES ARE AVAILABLE FROM DAYTON SUPERIOR. THE MATERIAL FOR THIS LIFTING SYSTEM ARE NOT INCLUDED IN THE BILL OF MATERIAL BUT ARE TO BE ORDERED AS REQUIRED.



GUIDE LINE DETAIL

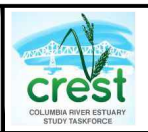
TYPICAL AT EACH END OF CAP
* SEE PRECAST CONCRETE CAP PLANS FOR LOCATION

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NO.	BY	DATE	REVISION DESCRIPTION

GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

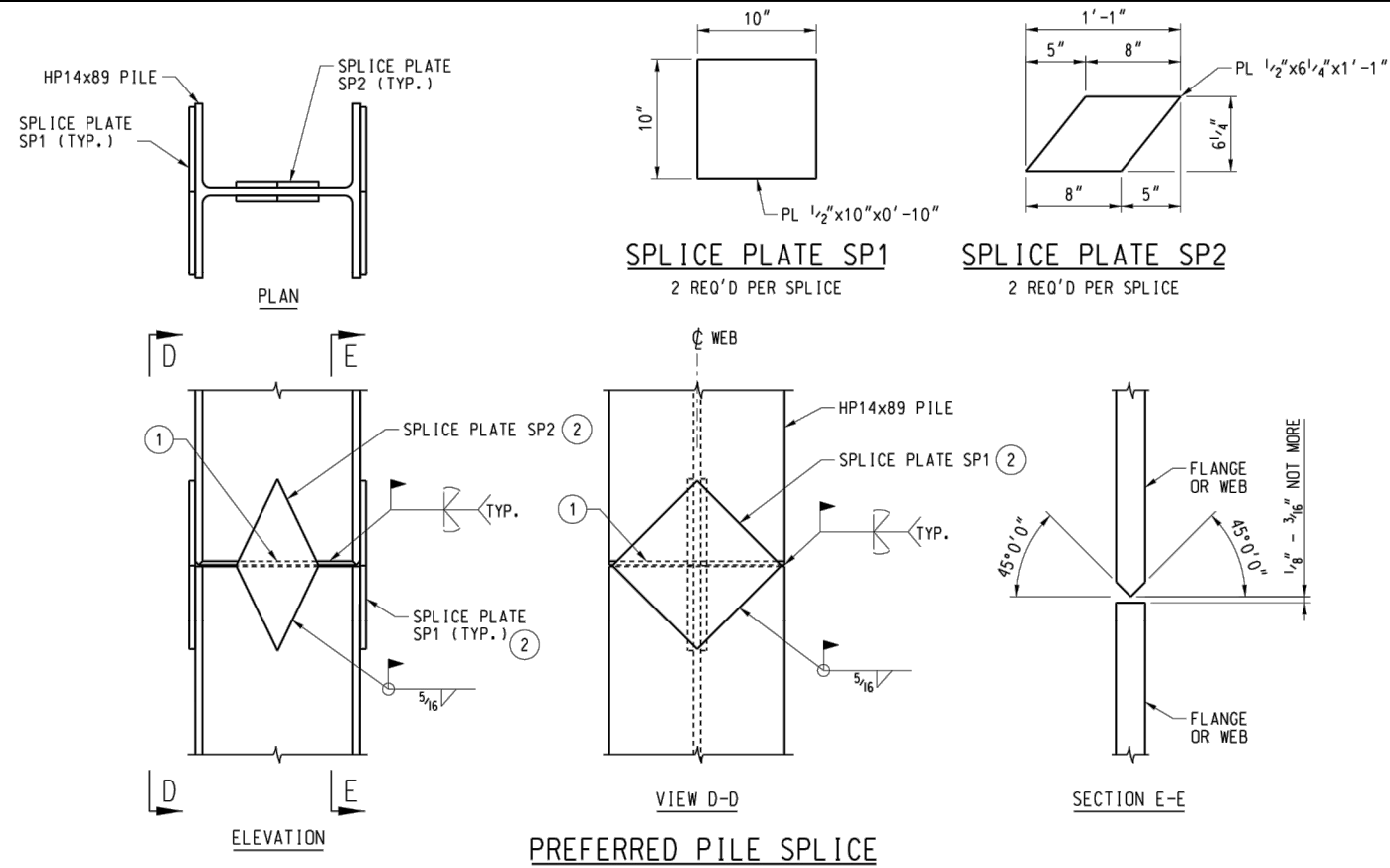
COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



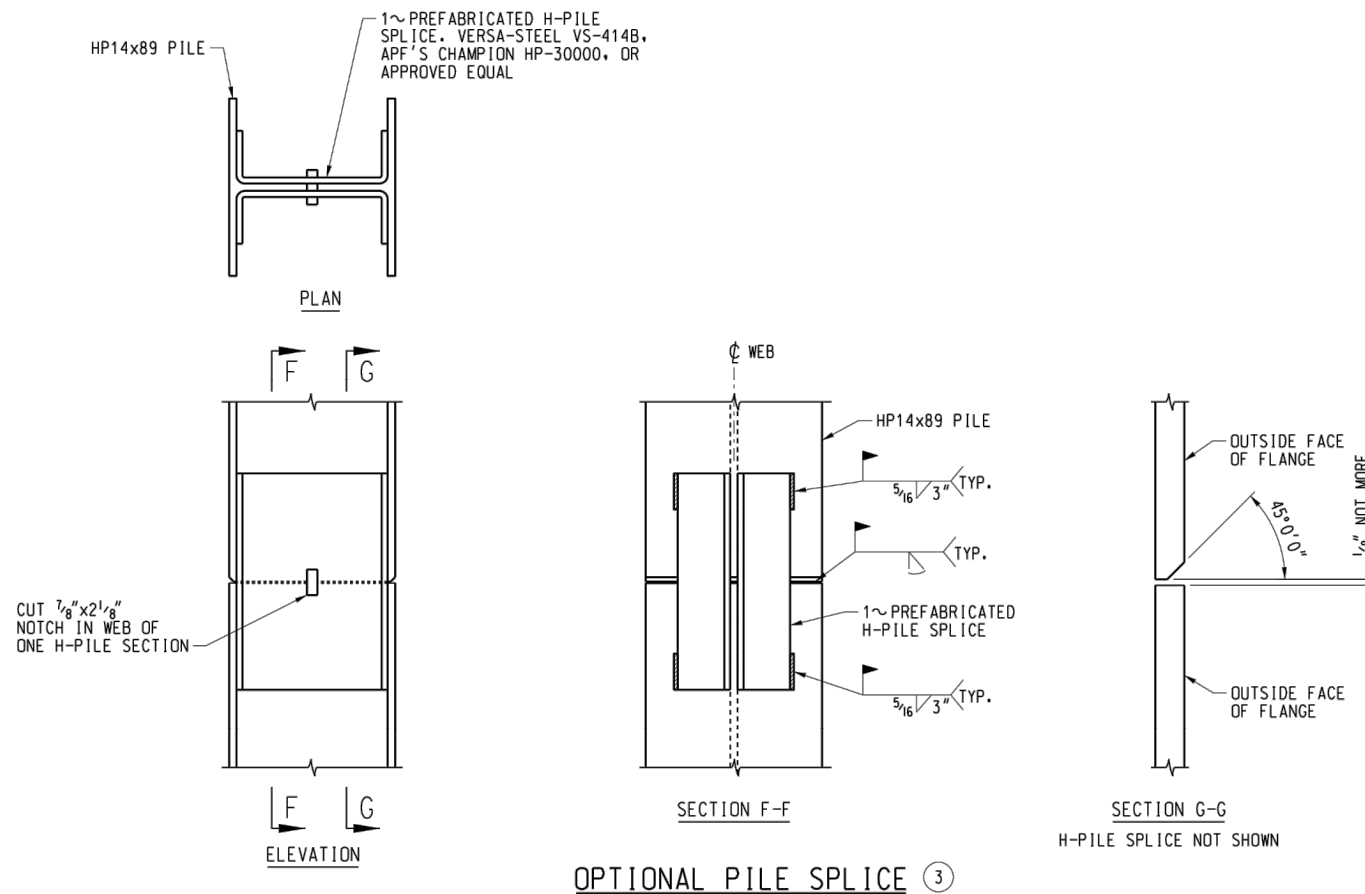
BRIDGE STANDARDS - LIFTING
DETAILS

SHEET
35 OF 40

Z:\ClientFiles\0-T-Railroad-Agency\Warren\Stouth_CREST_190220\Drawings\LEI_AGENCY_CREEK\LEI_RAILROAD_Agency_D.dwg - rnshear - 2/19/24



- ① BEFORE REINFORCING PLATES ARE INSTALLED, GRIND THE WELD CONTOURS WHICH WILL BE COVERED BY THE REINFORCING PLATES.
- ② REINFORCING PLATES ARE REQUIRED WHEN PILE SPLICE IS WITHIN 15 FEET OF THE FINISHED GROUNDLINE.
- ③ PREFABRICATED PILE SPLICES, IF USED, SHALL BE LOCATED A MINIMUM OF 15 FEET BELOW FINISHED GROUNDLINE.

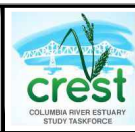


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NO.	BY	DATE	REVISION DESCRIPTION

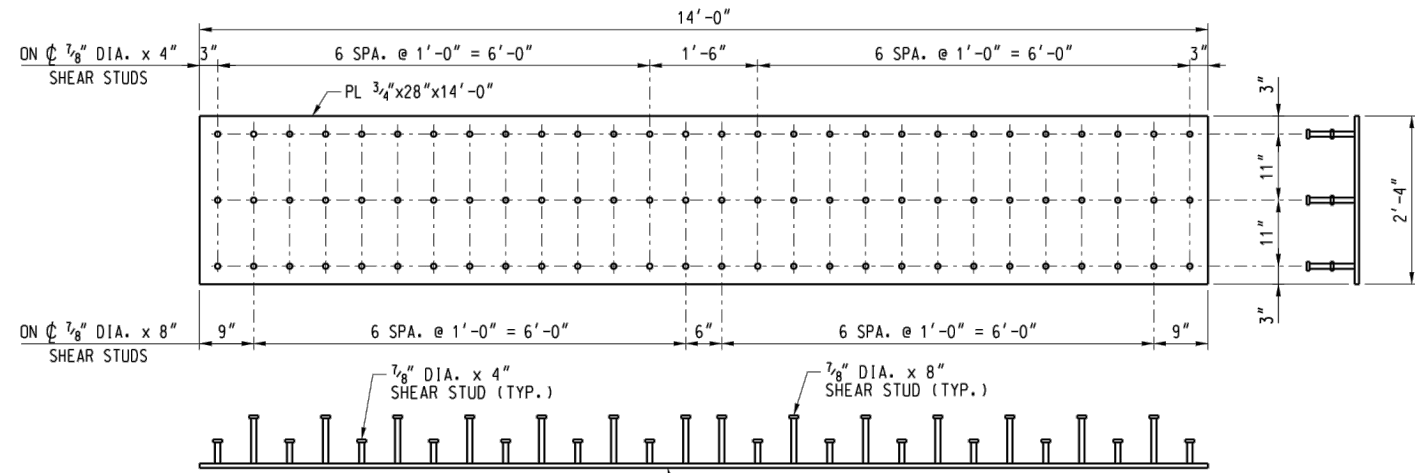
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN

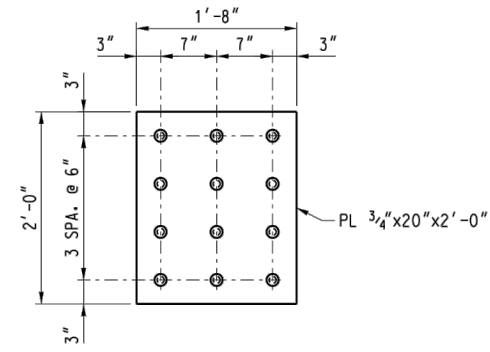


BRIDGE STANDARDS - PILE
SPLICE

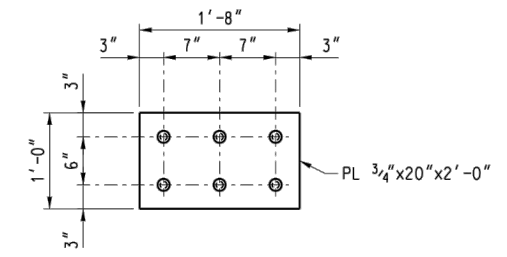
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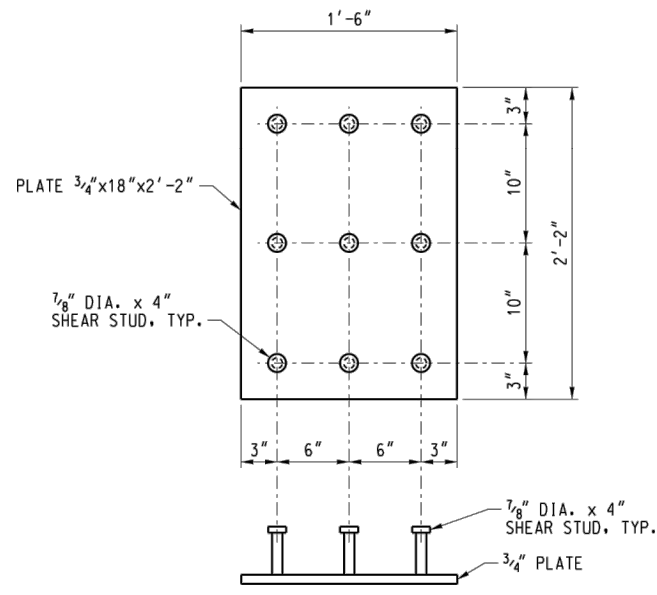
EMBED PLATE EP1
WEIGHT - 1097 LBS.



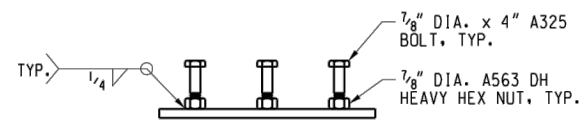
EMBED PLATE EP2
WEIGHT - 120 LBS.



EMBED PLATE EP3
WEIGHT - 60 LBS.

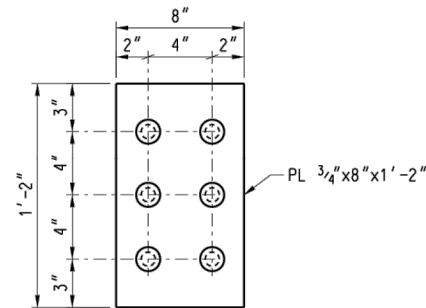


PREFERRED SHEAR STUD DETAIL



OPTIONAL SHEAR STUD DETAIL

EMBED PLATE EP4
WEIGHT - 107 LBS.



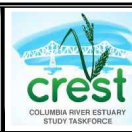
EMBED PLATE EP5
WEIGHT - 33 LBS.

80% SUBMITTAL
NOT FOR CONSTRUCTION

NO.	BY	DATE	REVISION DESCRIPTION

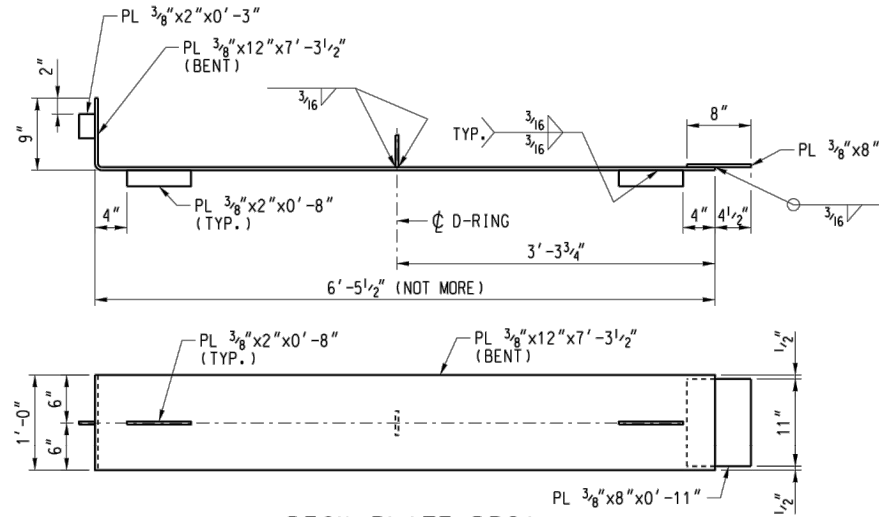
GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



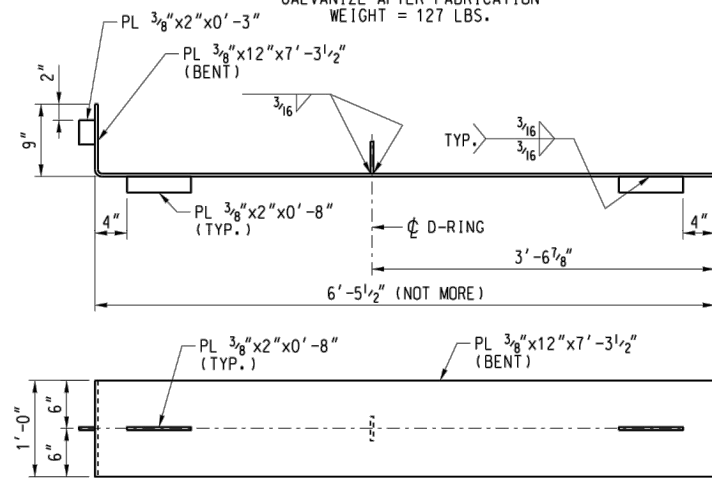
BRIDGE STANDARDS -
EMBEDDED PLATES

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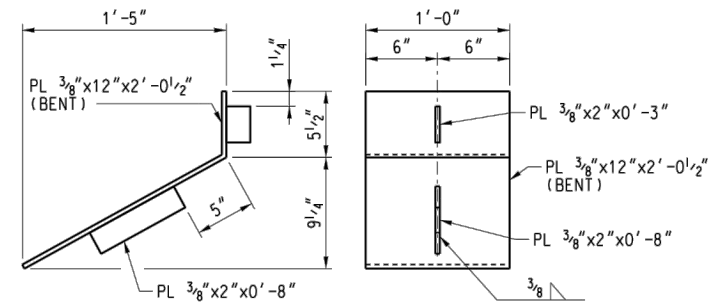
DECK PLATE DPS1

GALVANIZE AFTER FABRICATION
WEIGHT = 127 LBS.



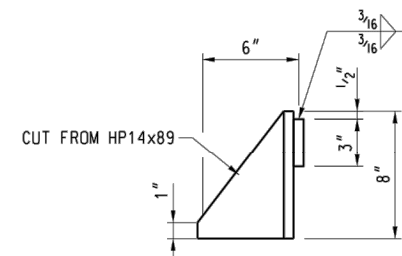
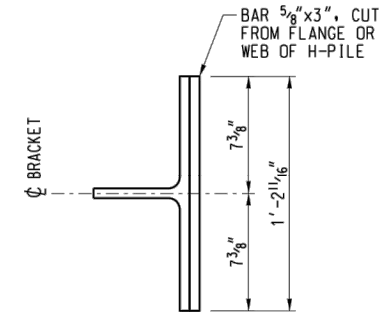
DECK PLATE DPS2

GALVANIZE AFTER FABRICATION
WEIGHT = 117 LBS.



DECK PLATE DPS3

GALVANIZE AFTER FABRICATION
WEIGHT = 35 LBS.



RETAINER BRACKET B100

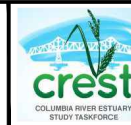
WEIGHT = 30 LBS.

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NO.	BY	DATE	REVISION DESCRIPTION

GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN

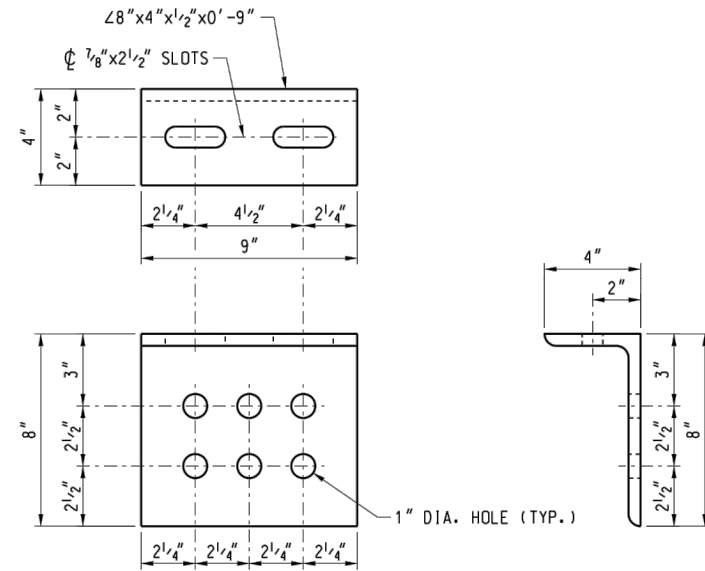


BRIDGE STANDARDS - DECK
AND CURB PLATES

SHEET

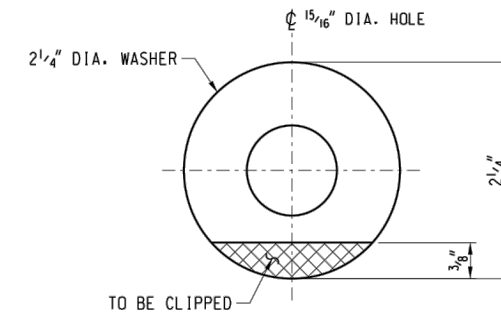
38 OF 40

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HANDRAIL BASE PLATE HBP2

GALVANIZE AFTER FABRICATION
WEIGHT = 15 LBS.



CLIPPED WASHER

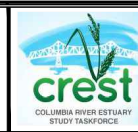
1 5/16" I.D. x 2 1/4" O.D. WASHER

80% SUBMITTAL
NOT FOR CONSTRUCTION

NO.	BY	DATE	REVISION DESCRIPTION

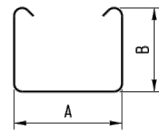
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MC APPROVED	02/2024 DATE	190220 PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN

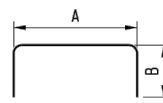


BRIDGE STANDARDS - CURB
AND WALK MISC.

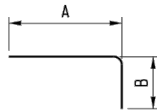
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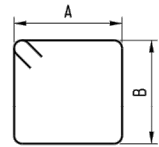
BAR A



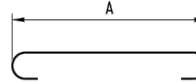
BAR E



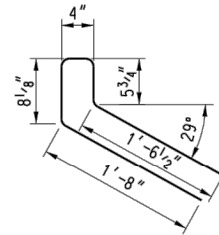
BAR J



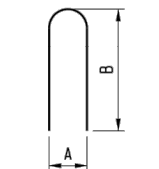
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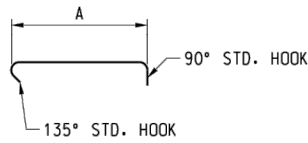
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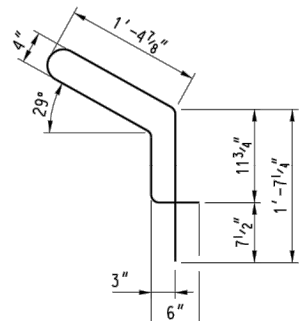
BAR K



BAR C

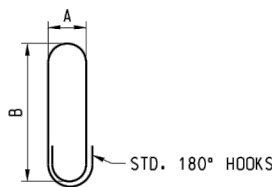


BAR G

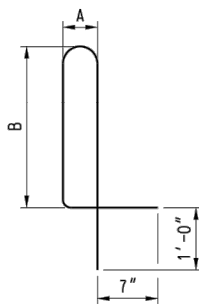


BAR L

A1035 BARS

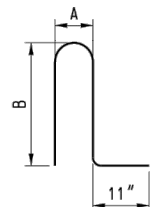


BAR D



BAR H

A1035 BARS



BAR M

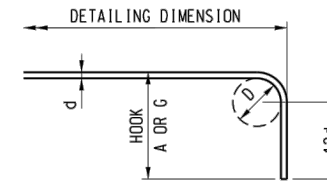
NOTES:

- REINFORCING STEEL IS TO BE IDENTIFIED PER THE FOLLOWING EXAMPLE:

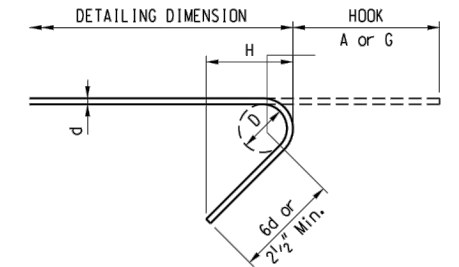
BAR NAME	BAR SIZE	DETAIL
424-4	#4	24'-4"
520	#5	20'-0"
409-11	#4	9'-11"
4C1	#4	G IS SHAPE OF BAR. 1 IS THE FIRST BENT BAR OF THIS TYPE.
- DIMENSIONS OF BENDING DETAILS ARE OUT TO OUT OF BAR.

STIRRUP & TIE HOOK DIMENSIONS

BAR SIZE	d (IN.)	D (IN.)	90° HOOK		135° HOOK	
			HOOK A OR G	HOOK A OR G	HOOK A OR G	H
#3	3/8"	1 1/2"	4"	4"	2 1/2"	2 1/2"
#4	1/2"	2"	4 1/2"	4 1/2"	3"	3"
#5	5/8"	2 1/2"	6"	5 1/2"	3 3/4"	3 3/4"
#6	3/4"	4 1/2"	1'-0"	8"	4 1/2"	4 1/2"
#7	7/8"	5 1/4"	1'-2"	9"	5 1/4"	5 1/4"
#8	1"	6"	1'-4"	10 1/2"	6"	6"



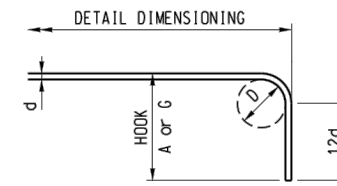
90° STIRRUP HOOK
(TIES SIMILAR)



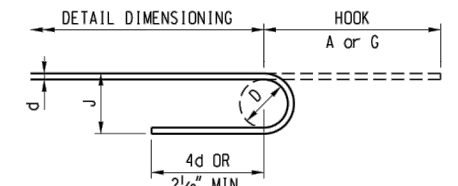
135° STIRRUP HOOK
(TIES SIMILAR)

STANDARD HOOK DIMENSIONS

BAR SIZE	d (IN.)	D (IN.)	90° HOOK		180° HOOK	
			HOOK A OR G	HOOK A OR G	HOOK A OR G	J
#3	3/8"	2 1/4"	6"	5"	3"	3"
#4	1/2"	3"	8"	6"	4"	4"
#5	5/8"	3 3/4"	10"	7"	5"	5"
#6	3/4"	4 1/2"	1'-0"	8"	6"	6"
#7	7/8"	5 1/4"	1'-2"	10"	7"	7"
#8	1"	6"	1'-4"	11"	8"	8"
#9	1 1/8"	9 1/2"	1'-7"	1'-3"	11 3/4"	11 3/4"
#10	1 1/4"	10 3/4"	1'-10"	1'-5"	1'-1 1/4"	1'-1 1/4"
#11	1 3/8"	12"	2'-0"	1'-7"	1'-2 3/4"	1'-2 3/4"
#14	1 3/4"	18 1/4"	2'-7"	2'-2"	1'-9 3/4"	1'-9 3/4"
#18	2 1/4"	24"	3'-5"	2'-11"	2'-4 1/2"	2'-4 1/2"



90° HOOK



180° HOOK

80% SUBMITTAL
NOT FOR CONSTRUCTION

NO.	BY	DATE	REVISION DESCRIPTION

GS DRAWN	MC, CA, MB DESIGNED	MB CHECKED
MC APPROVED	02/2024 DATE	190220 PROJECT

COLUMBIA RIVER ESTUARY STUDY TASKFORCE
RAILROAD RESTORATION
AGENCY CREEK - FINAL DESIGN



BRIDGE STANDARDS - REAR
BENDING DIAGRAM

SHEET
40 OF 40